



NORTH COFFS



DEVELOPMENT CONTROL PLAN



CONTENTS

PART 1 – INTRODUCTION	1
PREAMBLE	1
AMENDMENTS	1
RELATIONSHIP WITH OTHER DCP'S	1
OBJECTIVES	1
HOW TO USE THIS DCP	2
PROCEDURES	2
PART 2 – PLANNING STRATEGY: MASTERPLAN	3
OBJECTIVES	3
PRECINCTS	3
ESD STRATEGIES	3
URBAN DESIGN STRATEGY	4
PRECINCT SPECIFIC STRATEGIES	6
STORMWATER MANAGEMENT STRATEGY	9
INDICATIVE TREATMENT MEASURES	9
WATER SUPPLY SERVICING STRATEGY	10
SEWERAGE SERVICING STRATEGY	10
COMMERCIAL AND COMMUNITY SERVICE STRATEGY	14
TRAFFIC AND TRANSPORT STRATEGY	14
NATURAL ENVIRONMENT STRATEGY	14
OPEN SPACE AND RECREATION STRATEGY	15
PART 3 - PLANNING CONTROLS	18
HEIGHT	18
BUILDING DESIGN	18
HOUSING MIX	18
URBAN DESIGN	18
URBAN CAPABILITY, SOIL EROSION AND LANDSLIP	18
WATER QUALITY	19
ENERGY EFFICIENT DESIGN	19
POTENTIALLY CONTAMINATED LAND	19
BANANA LANDS BUFFERS	20
CULTURAL HERITAGE	20
NATURAL AND HABITAT AREAS	20
BUSHFIRE HAZARD	20
NOISE AND AIR QUALITY	21
ROAD DESIGN AND ACCESS CONTROL	21
PEDESTRIAN AND CYCLEWAY PATHS	22
SITE ANALYSIS	22
DEVELOPMENT PRECINCTS	22
COMMUNITY OPEN SPACE AND ENVIRONMENTAL CONSERVATION AREAS	23
PART 4 – GENERAL DEVELOPMENT CONTROLS	25
RESIDENTIAL LOW DENSITY	25
OBJECTIVES	25
SITE ANALYSIS	25
CONTROLS	26
SETBACKS	26
SOLAR ACCESS	28
PRIVATE OPEN SPACE	28
VEHICLE ACCESS AND PARKING	29
DEVELOPER CONTRIBUTIONS	29
EROSION AND SEDIMENT CONTROL	29
SERVICES	30
RESIDENTIAL MEDIUM DENSITY	31
OBJECTIVES	31

SITE ANALYSIS	31
DEVELOPMENT CONTROLS	32
SETBACKS.....	32
SOLAR ACCESS	32
PRIVATE OPEN SPACE	32
LANDSCAPING	32
ACCESS AND VEHICLE PARKING.....	33
EROSION AND SEDIMENT CONTROL.....	33
SERVICES	34
RESIDENTIAL TOURIST ACCOMMODATION	35
OBJECTIVES	35
SITE ANALYSIS	35
DEVELOPMENT CONTROLS	36
SETBACKS.....	36
SOLAR ACCESS	36
PRIVATE OPEN SPACE	36
LANDSCAPING	36
ACCESS AND VEHICLE PARKING.....	36
DEVELOPER CONTRIBUTIONS.....	37
EROSION AND SEDIMENT CONTROL.....	37
SERVICES	37
BUSINESS LANDS	39
OBJECTIVES	39
SITE ANALYSIS	39
BUILDING HEIGHT.....	39
BUILDING DESIGN.....	39
DENSITY	39
SETBACKS.....	40
HERITAGE	40
PARKING AND ACCESS	40
LANDSCAPING	40
LOADING / UNLOADING BAYS	41
OFF-SITE INFRASTRUCTURE REQUIREMENTS	41
SERVICES	41
OFF-STREET CAR PARKING.....	41
OBJECTIVES	41
DESIGN AND SAFETY	41
LANDSCAPING	41
PARKING SPACES.....	42
VISITOR / OVERFLOW CAR PARKING REQUIREMENTS FOR ALL RESIDENTIAL DEVELOPMENT	42
ACCESS.....	43
SCHEDULE A - PARKING REQUIREMENTS	44
GLOSSARY	46
SUBDIVISION	47
OBJECTIVES	47
SITE ANALYSIS	47
SERVICES	48
STREET TREE MASTERPLAN	49
DEVELOPER CONTRIBUTIONS.....	49
SIGNS.....	50
OBJECTIVES	50
PROCEDURES.....	50
SIGNS NOT REQUIRING CONSENT - RESIDENTIAL.....	50
SIGN DICTIONARY	50

SIZE OF SIGNS.....	51
EXISTING SIGNS	51
RESIDENTIAL SETTINGS	52
NOTIFICATION.....	53
OBJECTIVES	53
WHEN WILL DEVELOPMENT BE NOTIFIED / ADVERTISED?.....	53
CONTAMINATED LAND	53
FORM OF NOTICE / ADVERTISEMENT	54
CIRCUMSTANCES IN WHICH NOTICE REQUIREMENTS MAY BE DISPENSED WITH.....	54
NOTICE OF DETERMINATION	54
NOTIFICATION / ADVERTISING FEE	54

PART 1 – INTRODUCTION

PREAMBLE

- This Development Control Plan (DCP) applies to lands identified as Development Areas as shown in **Map 1 Masterplan**.
- This plan came into force on 1 June 2012.

AMENDMENTS

- This Plan has not been amended.

RELATIONSHIP WITH OTHER DCP'S

- This DCP repeals any other DCP made before this DCP that applies to the subject land.
- If there is an inconsistency between this DCP and any other DCP, whether made before or after the commencement of this DCP, this DCP prevails to the extent of the inconsistency.

OBJECTIVES

The controls in this DCP seek to achieve the following objectives.

Economic Sustainability

- To ensure that the existing community is not burdened by the provision of public utilities and facilities required as a result of future development.
- To equitably provide public utilities in a timely, cost-efficient and effective manner.
- To ensure development contributes to the economic growth of the City and provides links to local employment opportunities.
- To ensure that any commercial development acts to support the City Centre as the primary commercial district of Coffs Harbour and does not negatively affect the functioning of the City Centre.

Social Sustainability

- To provide safe and attractive neighbourhoods that meet the diverse needs of the community and offer a wide choice of housing and leisure opportunities, as well as associated community facilities.
- To improve the accessibility of public transport services and provide infrastructure to encourage walking and cycling, and to lessen the dependence on cars.
- Development is to create a sense of place for residents, respecting the natural setting and Aboriginal archaeological values of the landscape.

Environmental Sustainability

- To ensure development complements the conservation of biodiversity by protecting areas of high conservation value.
- To ensure that development takes into account environmental constraints including soil erosion, flooding and bushfire risk, and protects areas of natural and cultural significance.
- To ensure that development incorporates best practice urban water management techniques relating to stormwater quality and quantity, water conservation and re-use and ecosystem health.
- To ensure that development within the Coastal Zone is consistent with the aims and objectives of State Environmental Planning Policy 71 – Coastal Protection.
- To ensure development protects and maintains natural bushland, native habitats and koala populations, including the revegetation of linkages between habitats.
- To ensure development leads to maintenance or improvement in water quality that ensures healthy estuaries, for natural systems, residents and tourists.
- To ensure development of the area is energy efficient and minimises greenhouse gas emissions.

HOW TO USE THIS DCP

The DCP is divided into four parts:

- **Part 1: Introduction:** This part explains the purpose, objectives and how to use this DCP.
- **Part 2: Planning Strategy:** Masterplan describes the overall planning strategy for the area.
- **Part 3: Planning Controls:** Details the development rules and guidelines which apply to the area.
- **Part 4: General Development Controls:** Including general building, subdivision, environmental and servicing guidelines for development within the area.

Applicants are to comply with the overall Planning Strategy and detailed planning and general development controls unless it can be demonstrated that, in the opinion of Council, an alternative to any or all of the controls will meet the objectives.

PROCEDURES

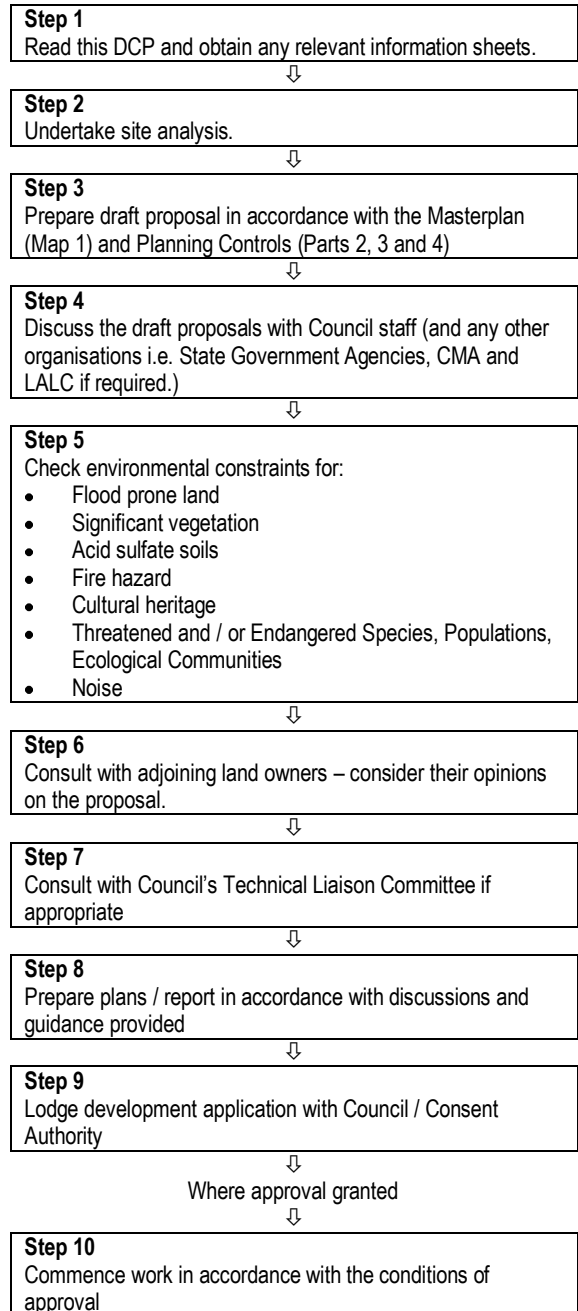
Part 2 provides an overall plan for the identified development areas area that will enable new development to integrate with existing development, services, traffic network and the surrounding environment.

Part 3 of this DCP contains the specific controls applying to Development within the North Coffs Development Area.

Part 4 of this DCP provides more detailed development controls for activities outside the scope of Parts 2 and 3.

Development generally requires the consent of Council unless it is exempt from approval. This consent is sought through the lodgement of a development application or complying development application. For all development requiring consent, applicants should follow the step-by-step process shown in the procedures flow chart. For all development that is eligible for assessment as Complying Development, applicants should refer to the relevant environmental planning instrument to ensure compliance.

PROCEDURES FLOW CHART



PART 2 – PLANNING STRATEGY: MASTERPLAN

OBJECTIVES

- To provide the basis for achieving the environmental, social, and economic objectives listed in Part 1 of this DCP.
- To provide an overall plan to enable the coordination of existing and new development.
- To provide for quality residential and tourist precincts that provide attractive 'mixed density' development, with enhanced landscaping, that respects local view corridors while maximising the amenity gained from views out from the site.
- Assist in achieving a Compact City for Coffs Harbour by providing for an appropriate future density within the North Coffs Release Area.

PRECINCTS

The North Coffs Release Area has been divided into four discrete precincts:

- *R1 medium density precinct within the middle of the southern area of the Release Area;
- *R2 medium density precinct on the south-eastern area of the Release Area adjacent to the existing bulky goods / retail precinct;
- R3 / T1 medium density precinct incorporating a tourism component in the north-eastern (Big Banana) area; and
- R4 Low Density Precinct known as the Pacific Bay - Western Lands.

*The R1 and R2 Precincts are deferred and are subject to further investigation.

These precincts can be identified on the site masterplan (refer to **Map 1**).

Each precinct will have its own strong sense of identity with clearly defined boundaries. Existing vegetation, steep and elevated land, water courses and transport corridors (the North Coast Railway, the existing or proposed new Pacific Highway alignment or existing local roads) will separate the precincts from each other and surrounding areas.

Development within each precinct will be designed to optimise local setting attributes, such as distant

views, while complimenting and protecting other more sensitive visual and physical site features such as steep slopes, drainage lines, remnant vegetation and ridgelines.

ESD STRATEGIES

Economic Sustainability: Objectives

- Assist in accommodating the predicted increase in population within Coffs Harbour by providing for an additional population in excess of 1000 people.
- Enable sustainable residential development and tourist development and accommodation opportunities.
- Enable development that supports the continuation of the Big Banana as a Major Tourist Attraction to the Coffs Harbour LGA.
- Require developers to provide reticulated water and sewer to new development by extending the existing network.
- Require developers to forward fund any works required ahead of Council's Developer Servicing Plans.

Social Sustainability: Objectives

- Construction of internal and collector roads, in accordance with the Masterplan will generally be the responsibility of individual developers and in accordance with the North Coffs Developer Contribution Plan.
- Developers will be required to provide pedestrian access in a timely manner, linking residential areas to the public open space network.
- Developers will be required to provide bus shelters so that all residential areas are generally located within five hundred metres from the nearest shelter.
- Developers will be required to provide children's playgrounds so that all residential areas are no further than five hundred metres from the nearest facility.

- Provision of useable public open space in accordance with Council's Open Space and Recreation Strategy.
- Place name selection will include consultation with the local Aboriginal Land Council to determine if there is an appropriate aboriginal place name.

Environmental Sustainability: Objectives

- All development will have regard to the need for quality environmental management.
- Restrict development from environmental protection areas and require the dedication of such land to Council as part of development proposals at no further cost to Council.
- Ensure development proposals provide bushfire buffers that do not encroach on environmental protection areas or involve the removal of native vegetation.
- Encourage the use of bushfire buffers for passive recreational activities and / or community facilities where it does not involve negative impacts on environmental values.
- Utilise best practice water sensitive urban design strategies to reduce nutrient loading, sedimentation and chemical contamination of local waterways.
- Restrict urban development from land identified with a significant flood risk.
- Utilise acoustic design to ensure highway traffic, rail and industrial noise does not exceed acceptable levels within identified residential areas.
- Provide landscaped buffers (utilising local native species) between future residences and the highway and the rail corridor and where practical incorporate essential service corridors.

URBAN DESIGN STRATEGY

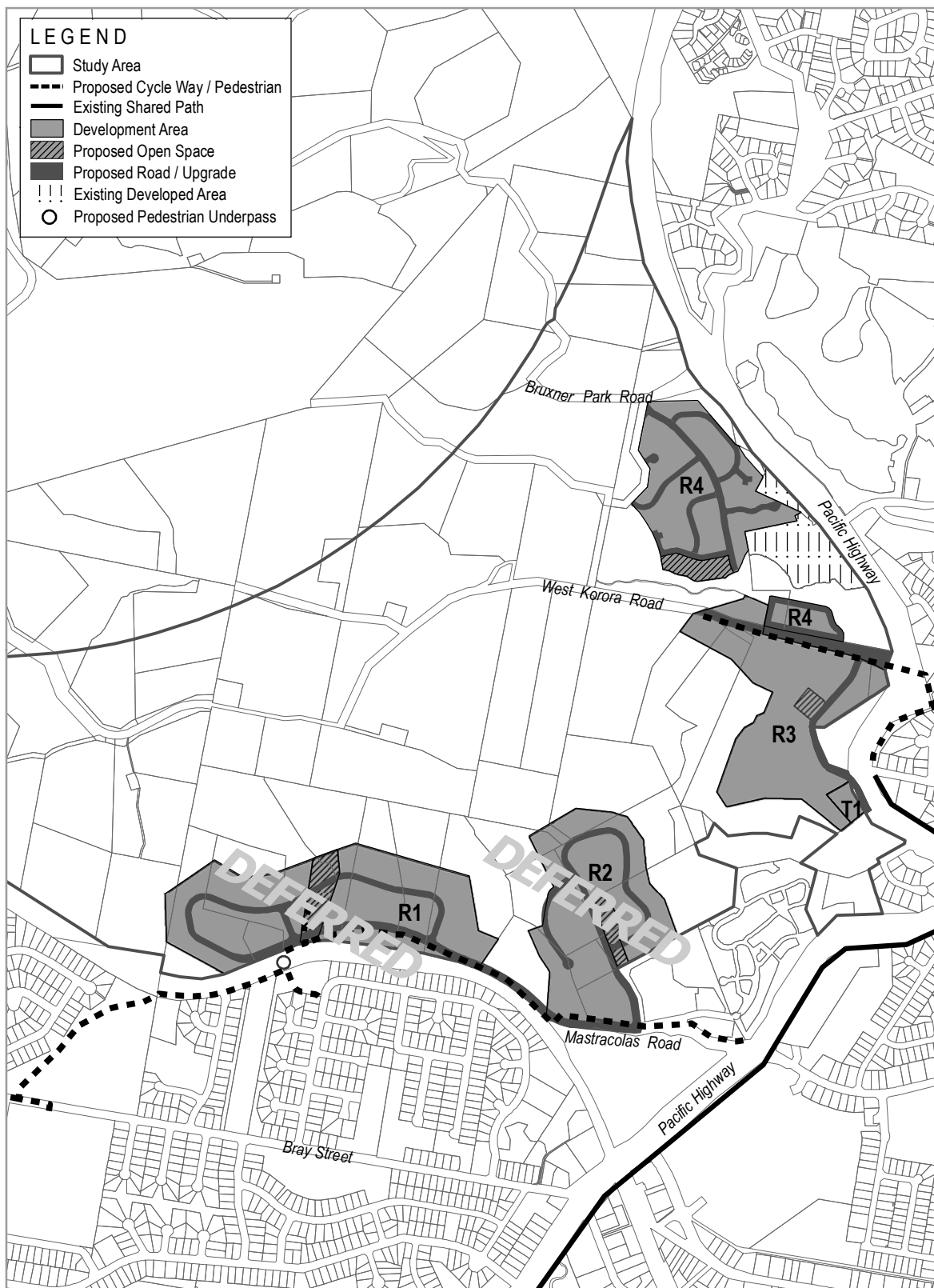
General Strategies

- A variety of land uses will be accommodated within the locality, including environmental conservation, open space and rural purposes, low density to medium density residential development, tourist facilities.
- A variety of lot sizes and mix of densities shall be provided to facilitate housing diversity and choice

to meet the requirements of people with different housing needs.

- Neighbourhoods shall be pedestrian focused ensuring adequate pedestrian links to adjoining residential, commercial, community and education services, ensuring permeability and legibility within and between precincts.
- Development shall provide a healthy and active lifestyle setting that encourages walking and other non-vehicular forms of access.
- Development shall provide for connection to existing or proposed development on adjacent sites.
- A diverse range of public facilities and recreational opportunities shall be provided by developers that establishes a desirable community lifestyle and strong sense of identity.
- Urban form is to ensure a site-responsive approach that supports and enhances the context in which it is located, particularly the topography and environmental constraints of the land.
- Development shall strengthen local character and identity and promote a sense of community through a range of design techniques.
- Development shall optimise existing natural attributes to create a unique new community that sets itself apart from other conventional developments in the area.
- Development shall create an environment that offers a high sense of public safety through the principles of crime prevention through environmental / urban design.
- Development shall provide open space linkages to adjacent environmental protection and / or open space areas.
- Residential development utilising 'gated' street formats is strongly discouraged.
- Building siting and design shall enhance solar access and incorporate climatic considerations.
- Provide built form and an urban design character that is complimentary with the natural and rural context of the site.
- Optimise water management systems that are complimentary with the visual and recreation objectives.

MAP 1 - MASTERPLAN



North Coffs
Development Control Plan

Map 1 - Masterplan

N
1:15,000

PRECINCT SPECIFIC STRATEGIES

R1 PRECINCT (DEFERRED)

Overview

Development within the R1 precinct will be residential, of a medium density. In the flatter areas, dwellings of up to two storeys on smaller lot sizes are suitable. As this Precinct is in proximity to the Homebase and Park Beach Plaza retail and commercial areas, increased residential densities are appropriate to ensure the most efficient use of this land. On the steeper land, larger lot sizes are more suitable, to ensure visual impact is minimised.

The street environment of this neighbourhood will have a strong urban character. The smaller lots are permitted to have reduced building setbacks from street boundaries and tighter external spaces at the rear and sides of buildings. The neighbourhood will have a legible and interconnected system of streets and footpaths that provide safe access for residents while encouraging walking and cycle activities.

Residents have easy access to large areas of open space located across the railway to the south west. Access will be improved with a new pedestrian path, which links this Precinct to the existing urban area to the South (across or under the existing railway).

The street pattern will incorporate a series of interconnected roads.

Central to the layout should be an area of medium density residential development. This could be located along the railway, oriented northwards and appropriately designed to reduce noise for occupants.

Adjacent to this should be a general store servicing the convenience and incidental consumption needs of the surrounding residential population. This store may be located adjacent to a passive public open space which will cater for the passive recreational needs of the local community and provide for a focal meeting place. This open space area may contain playgrounds, informal open grass areas and sheltered seating facilities.

The open space network will incorporate water management systems where water sensitive urban design elements such as bioretention gardens and wetlands establish a visually and ecologically rich landscape setting.

Possible community facilities and services include:

- a general store;
- a parkland adjacent to the small commercial node;
- small local parks of open grassland, perimeter planting and a seating niche; and

- walkways and bike paths.



Indicative Residential Character for the R1 Precinct

R2 PRECINCT (DEFERRED)

Overview

The R2 Precinct is a medium density residential precinct, located in a less visible and less sensitive part of the study area, providing the scope for a more intensive urban environment. This area will incorporate a mixture of single dwellings and multi-unit dwellings of up to three storeys, occupying smaller lot sizes to generate a relatively large residential population. This area will provide the population base that is not achievable in other areas of the site, due to environmental constraints.

Residents from this neighbourhood will benefit from the close proximity to the nearby commercial areas of 'Homebase' and 'Park Beach Plaza' which are within walking distance.

The street environment of this neighbourhood will have a stronger urban character than the other residential precincts. The smaller lot sizes may require reduced building setbacks from street boundaries to ensure adequate external spaces at the rear of buildings. Residents should benefit from proximity and easy access to areas of open space on the southern side of the Railway and also areas provided within the precinct.

This neighbourhood will contain areas of natural bushland that will provide it with an enduring visual and recreational resource.

The neighbourhood will have a legible and interconnected system of streets and footpaths that aim to provide safe access for residents while encouraging walking and cycle activities.

The recommended street pattern will be subject to the constraints (slope and shape of the precinct) but should incorporate a road system that provides for ease of

access for residents in terms of private vehicles, public transport and pedestrians.

Possible community facilities and services include:

- neighbourhood playground;
- passive recreation areas; and
- walkways and bike paths.



Indicative Residential Character for the R2 Precinct

R3 / T1 PRECINCT

Overview

The eastern portion of this site, in proximity to the Big Banana, is a residential precinct incorporating a tourism component. Within this precinct, the layout of development must be carefully configured to optimise and complement the visual constraints of the site generated by its elevated location, vegetation, and views to the north and east.

The layout of the precinct should be consistent with Development Control Plan (Big Banana / Summit Land) and any future development of the Big Banana Lands covered by the aforementioned DCP. Future building form will adopt the following principles:

- pitched, gabled or curved rooflines are encouraged, flat rooflines are to be avoided except when used as a minor design variation;
- to achieve diversity in building design, consideration will be given to elements such as verandas, awnings, balconies, window hoods, curved or pitched rooflines, and overall differentiation of top, middle and base of the building;
- inclusion of a mix of compatible materials;

- balcony enclosures will only be permitted where the overall design of the façade is not adversely affected;
- ridgelines should be replanted with native vegetation to reduce visual impact;
- the density of development will vary dependant on the site characteristics including slope and visual prominence; and
- buildings should be smaller in size, and occupy a small footprint to reduce the overall physical bulk of the development and to ensure optimal scope for vegetation between buildings.

Tourism could be an additional use within this precinct to optimise and complement the unique facilities that exist within the Big Banana site. New buildings should incorporate low scale built form and light weight materials to minimise visual impact while providing scope for a high quality, contemporary design.

The anticipated tourism facilities could include conferencing rooms, accommodation, resort facilities, and a range of recreational opportunities such as tennis, swimming, cycling and walking. These should be designed to visually integrate with the surrounding residential precinct and to form a strong connection with the Big Banana to strengthen and complement the facility and to encourage patronage of the facilities by residents and visitors.

Possible community facilities and services are:

- tourist and associated commercial facilities;
- rest and seating areas;
- parks and
- walkways and bike paths.



Indicative Tourism / Residential Character for the R3/T1 Precinct

R4 PRECINCT

Overview

The R4 precinct consists entirely of land known and referred to as the Pacific Bay - Western Lands and is to accommodate low density residential development as predominant land use. R4 precinct is located in the northern section of the North Coffs Release Area. The site slopes generally in a Northwest to Southeast direction to Jordan's Creek. An extensive remnant rainforest exists within the precinct which will be retained.

This area will incorporate a mixture of single one and two storey dwellings and townhouses, with varying lot sizes to offer a range of choices for new residents.

Residents from this neighbourhood will benefit from proximity and ease of access to the nearby Pacific Bay Resort facilities and services.

The street environment of this neighbourhood will have a strong residential urban character. Residents should benefit from proximity and easy access to areas of open space provided within the precinct including the retained sports field, local park and pedestrian pathways in and around environmental conservation areas.

This neighbourhood will contain areas of natural bushland that will provide it with an enduring visual and recreational resource.

The neighbourhood will have a legible and interconnected system of streets and footpaths that aim to provide safe access for residents while encouraging walking and cycle activities.

The recommended street pattern will be subject to the constraints (slope and shape of the precinct) but should incorporate a road system that provides for ease of access for residents in terms of private vehicles, public transport and pedestrians.

Community facilities and services will include:

- neighbourhood playground;
- passive recreation areas; and
- walkways and bike paths.



Indicative Residential Character for R4 Precinct

SERVICING STRATEGY

- Servicing will meet the needs of the present and future community whilst ensuring a healthy and sustainable built and natural environment.
- The provision and management of servicing shall be in accordance with “best practice” standards.
- Developers will be required to extend water and sewer from the nearest point identified in Council’s Water and Wastewater Developer Servicing Plans.
- Where services identified in Council’s Water and Wastewater Servicing Plans are not in place, it will be the developer’s responsibility to forward fund these services.
- Development shall enable efficient provision of utility services to new localities, and shall ensure that Water Sensitive Urban Design features be incorporated at the time of development.

STORMWATER MANAGEMENT STRATEGY

OBJECTIVES

Stormwater management will achieve the following objectives:

- alleviate downstream flooding in the Jordans and Coffs Creek catchments;
- protect water quality in all catchments;
- maintain natural flow regimes in the Jordans and Coffs Creek catchments;
- consider flooding in all future actions on the stormwater system; and
- no increase in gross pollutants in the Jordans and Coffs Creek catchments.

Stormwater management will complement related objectives for the site, including protecting and enhancing the extent of native riparian vegetation and integration with community open space.

The adopted strategy for the release area will be influenced by the characteristic steep slopes of the mid to upper portions of each catchment within the locality. The required treatment and detention will need to be focused more on ‘end-of-line’ measures in the flatter, lower sections of each catchment.

To assist in achieving detention requirements, rainwater tanks are also required on all allotments. Contemporary water sensitive urban design treatment measures are recommended for the stormwater management strategy for the site.

INDICATIVE TREATMENT MEASURES

Detention Basins

Downstream flooding will be alleviated by either construction of large detention basins in the development area or by increasing the capacity of the downstream drainage system. The detention basin option may involve reducing the post-development 100 or 500 year flow to a level equivalent to the current 20 year flow. It is estimated this will require detention basins with areas equivalent to 10% to 20% of the proposed development area. For some of the development areas it may be more advantageous to fund downstream drainage works to alleviate deficiencies in the downstream drainage capacity. This funding would be executed via a Contributions Plan. This decision will be based on analysis of the downstream drainage system.



Indicative Detention Basin

Rainwater Tanks

Rainwater tanks are required for all individual allotments for the purpose of both stormwater detention and reuse. The recommended minimum size of the tanks is 5 kL for standard residential allotments and 3 kL for attached dwellings associated with medium density areas.



Indicative Rainwater Tank

Bioretention Swales / Basins

Bioretention swales and basins are proposed as final treatment measures in open space areas in the downstream portions of some of the catchments. The

size of the swales / basins are based on providing a filter area equivalent to 2 % of the catchment area to achieve the pollutant reductions objectives. Therefore an allowance of 5 % of the developed catchment area is considered a reasonable average to accommodate the total footprint area of each bioretention swale / basin.



Indicative Bioretention Swale

Wetlands / Ponds

Wetlands may be a suitable option where ponds / farm dams currently exist. The existing ponds / dams would need modification to provide an inlet zone, sediment basin, macrophyte zone and high-flow bypass. The size of the wetlands is based on providing a filter area equivalent to 5 % of the catchment area to achieve the pollutant reductions objectives. Therefore an allowance of 8 % of the developed catchment area is considered a reasonable average to accommodate the total footprint area of the wetlands.



Indicative Wetland / Pond

WATER SUPPLY SERVICING STRATEGY

Map 2 indicates the preferred Reticulated Water Supply Strategy for the North Coffs Release Area.

Under the provisions of Council's Water Supply Strategy, Council cannot supply reticulated water services to land located above 55m AHD. The proposed development areas are generally below the 55 metre contour. Any application for development above this height requires the applicant to include provisions for water supply to those proposals.

PRECINCTS R1 AND R2 (DEFERRED)

For these precincts, a new trunk main is required, connected to the existing 600mm diameter trunk main behind the 'HomeBase' area. The size of the new trunk main will be up to 250mm diameter. The developable area of this zone is approximately 28ha with a corresponding peak demand of 28L/s.

Consideration has been given to extending existing 100mm diameter mains located on the southern side of the railway line. However due to the high elevation of the subject site, the headlosses in the 100mm diameter mains would most likely result in less than desirable pressures in the higher portions of this zone.

PRECINCTS T1 AND R3

This area will be serviced by branch lines from the existing 450mm diameter trunk main and extension of the existing 225mm diameter main located in Island View Close.

PRECINCT R4

This area will be serviced by the existing water supply mains located along the western frontage of the site, adjacent to the Pacific Highway.

SEWERAGE SERVICING STRATEGY

Map 3 indicates the preferred Sewerage Strategy for the North Coffs Release Area

PRECINCTS R1 AND R2 (DEFERRED)

The following sewerage infrastructure is required:

- a new 150mm gravity main to connect into an existing 225mm gravity main behind the 'HomeBase' area. It is recommended the new gravity main is located along the railway reserve to take advantage of the relatively even grade of

ground surface, thereby minimising excavation requirements for construction of the main; and

- upgrading the pumping capacity at Pumping Station No.13.

PRECINCTS T1and R3

The following sewerage infrastructure is required:

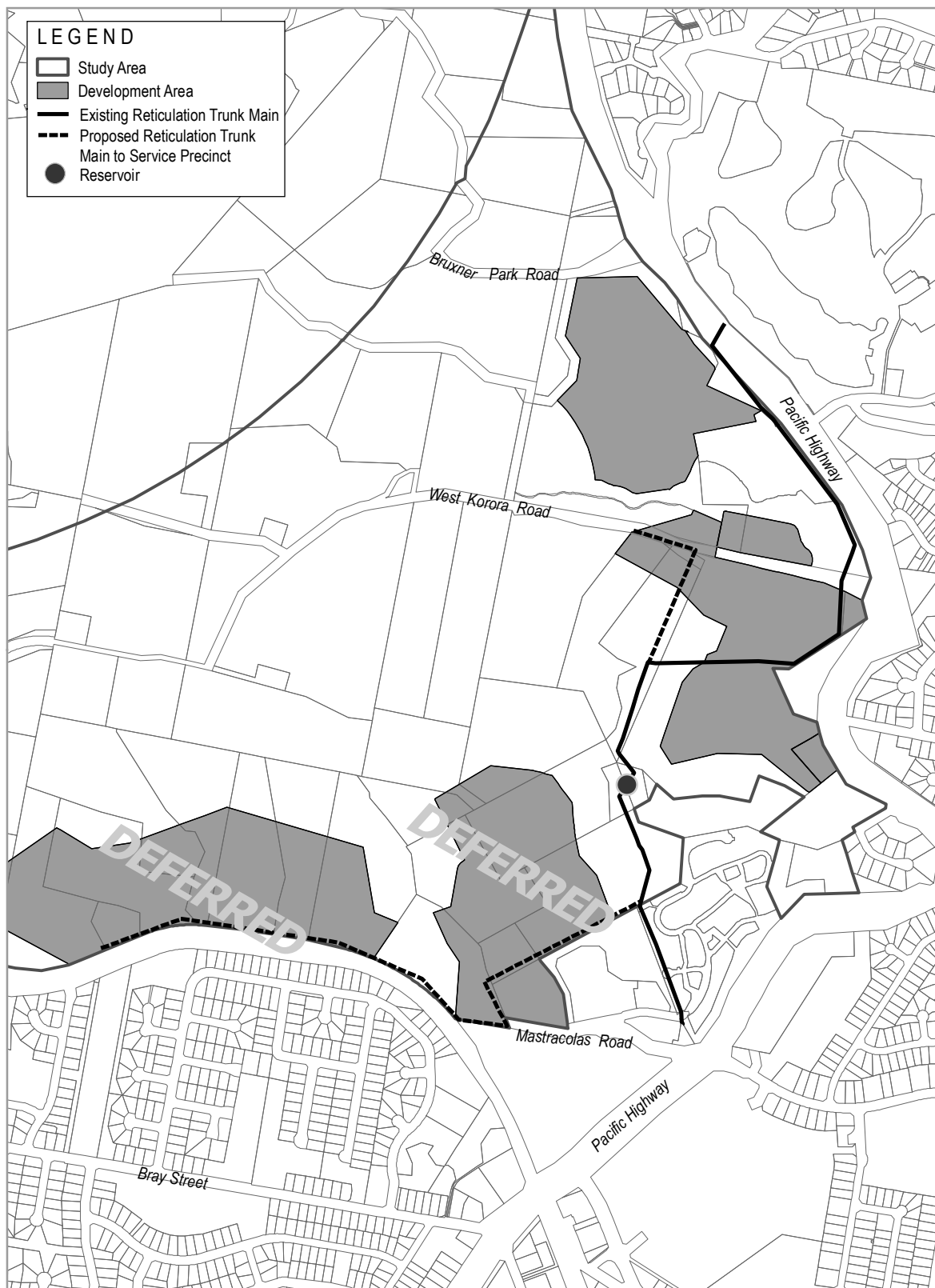
- a new 150mm gravity main along West Kororo Road from Precinct R3 to the existing 150mm gravity main from the Pacific Bay – Western Lands land near the intersection at the existing Pacific Highway;
- replacement of the existing 150mm diameter trunk main from the above junction point to Pumping Station No.8 with a 225mm diameter main (including underboring beneath the existing Pacific Highway);
- replacement of the existing 150mm diameter main servicing the Big Banana site with a new 225mm diameter trunk main; and
- upgrading the pumping capacity at Pumping Station No.8.

PRECINCT R4

The following sewerage infrastructure is required:

- a new 150mm gravity main along Bruxner Park Road and beneath the existing Pacific Highway to connect into an existing 150mm diameter main in the catchment of Pumping Station No. 61.

MAP 2 - WATER SUPPLY STRATEGY

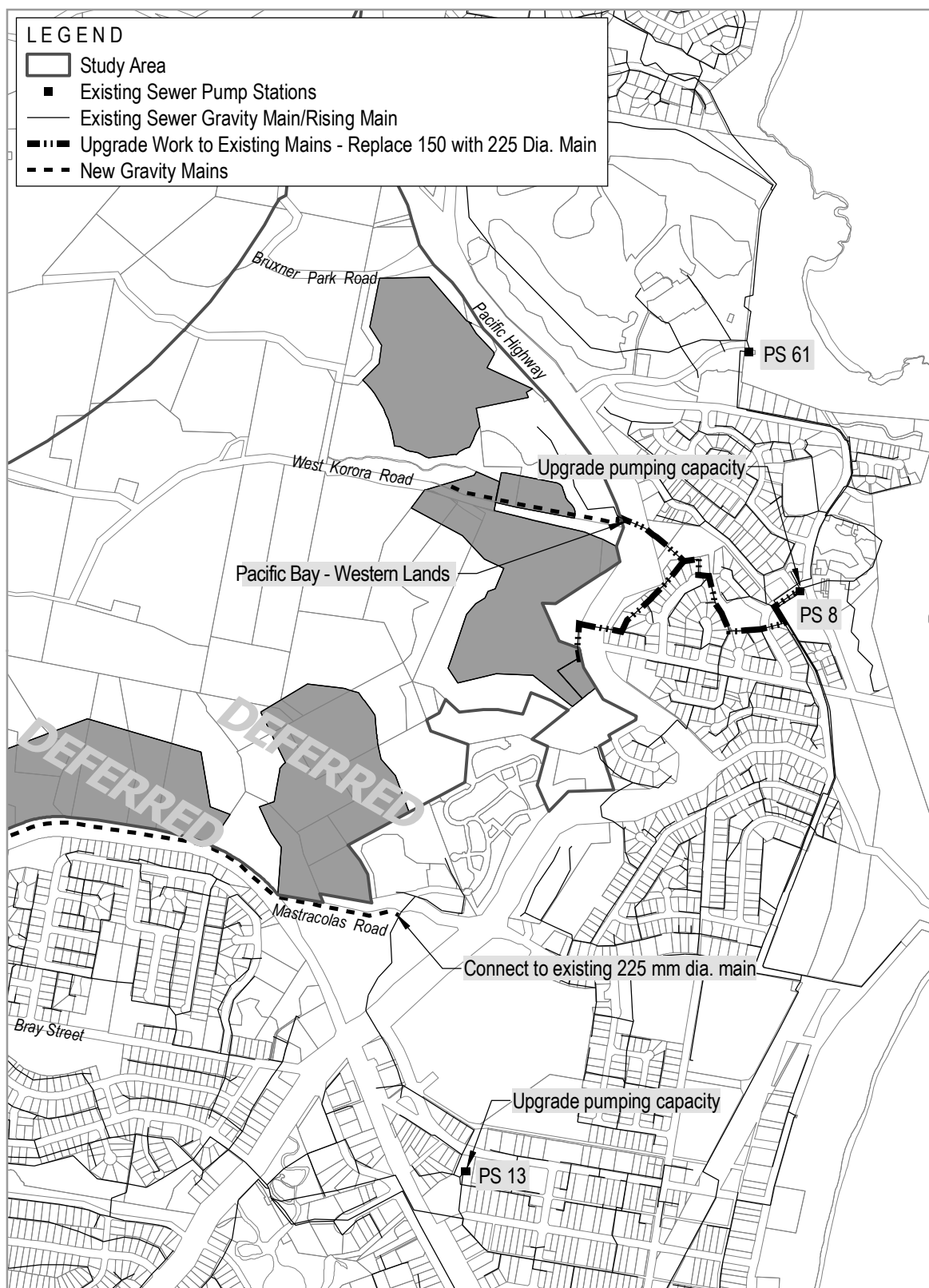


North Coffs
Development Control Plan

Map 2 -Water Supply Strategy

N
1:12,500

MAP 3 - SEWERAGE STRATEGY



North Coffs
Development Control Plan

Map 3 - Sewerage Strategy

N
1:15,000

COMMERCIAL AND COMMUNITY SERVICE STRATEGY

- Any commercial development will be generally limited to a general store in precinct R1 to service future residential development of the area and commercial development associated with the Big Banana Development and future tourist development. The existing commercial facilities in the locality (Homebase / Park Beach Plaza and other centres) and the Coffs Harbour Central Business District (CBD) will also service new development.
- Links between proposed precincts and the existing links to existing commercial nodes such as Park Beach, Home Base and Coffs Harbour CBD will need to be established.
- Community service provision shall evaluate the needs of the demographic and be responsive to demand.
- Urban form shall integrate and consolidate surrounding commercial and community services via existing road networks

TRAFFIC AND TRANSPORT STRATEGY

- Traffic management will achieve efficient and safe movement of people.
- Urban form will provide good accessibility through street layout and promote walking and cycling.
- Adequate transport and pedestrian linkages to community facilities and open space areas will be provided by the developer.
- Bus shelters to service the area to be funded by the developer.
- It will be the responsibility of the developer to:
 - extend local road access to and upgrade the identified collector road system in accordance with the North Coffs Developer Contributions Plan;
 - ensure pedestrian and cycleway linkage are provided to local schools, shops, playing fields and parks;
 - implement traffic calming devices at cycleway crossover points on the collector road systems; and
 - utilise minimum road and verge width to prevent impacts on roadside ecology.
- Preferred road and intersection locations are shown in **Map 4**.

Road infrastructure road works required to service the proposed precincts are summarised below:

- Precinct R1 and R2 (Deferred Areas):
 - Mastracolas Road will be required to be upgraded to service future development;
 - the existing grade separated intersection of the Pacific Highway and Mastracolas Road has sufficient spare capacity to accommodate additional traffic generated by development of the R1 and R2 precincts;
- Precinct T1 and R3 :
 - installation of traffic signals at West Korora Road / Pacific Highway intersection to cater for future urban development;
 - upgrade West Korora Road to service Precincts T1 and R3 ; and
 - provide for safe pedestrian access along West Korora Road across the Pacific Highway Intersection or by other appropriate means.

These works will need to be provided by the developer and in accordance with the North Coffs Developer Contribution Plan.

- Precinct R4:
 - Upgrade Bruxner Park Road / Pacific Highway intersection with either traffic signals or a roundabout being constructed as part of early works for the Coffs Harbour bypass.
 - Part of Precinct R4 will obtain access via West Korora Road. Any development will need to contribute in accordance with the North Coffs Developer Contribution Plan.

NATURAL ENVIRONMENT STRATEGY

- Regeneration of the riparian vegetation along the existing watercourses to overcome the effects of past grazing practices should be incorporated as part of new development.
- Protect and enhance environmental protection areas including bushfire buffers, creek buffers, development buffers, significant vegetation and other constrained land.
- Exclude urban development from sensitive areas to protect riparian vegetation, maintain water quality and provide habitat linkages.
- Protect and enhance high value vegetation.
- Exclude urban development from known habitat for endangered fauna species, and provide sufficient buffers to development.

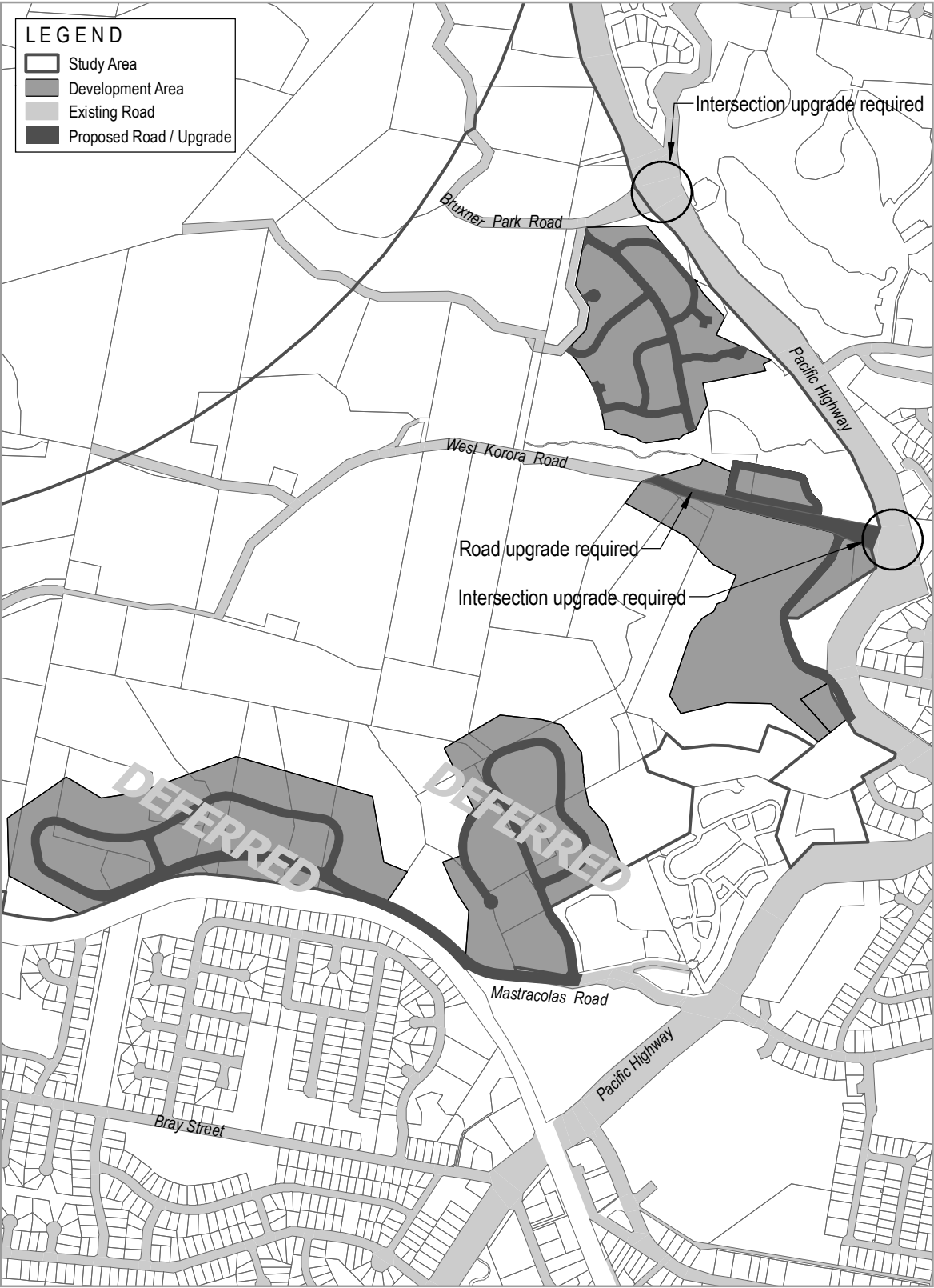
- Provide bushfire buffers between environmental protection areas and future development without the removal of high value vegetation.
- Obtain the dedication of land that cannot be developed due to the bushfire buffer requirements and incorporate into environmental protection areas at no further cost to Council.

OPEN SPACE AND RECREATION STRATEGY

- Neighbourhood open space will be located within 500m of all residences.
- Incorporate environmental protection areas into a network of public open space.
- Manage open space areas and community land in accordance with the relevant Plan of Management.
- Encourage passive recreation within open space areas (including bushfire buffers) where it does not involve the removal of native vegetation and / or damage to the environmental values of the area.

Map 5 outlines the proposed Open Space Network for the North Coffs Harbour Release Area.

MAP 4 - PREFERRED ROAD NETWORK



North Coffs
Development Control Plan

Map 4 - Preferred Road Network

N
1:12,500

MAP 5 - OPEN SPACE NETWORK



North Coffs
Development Control Plan

Map 5 - Open Space Network



PART 3 - PLANNING CONTROLS

HEIGHT

The maximum permissible building height will vary, based on the precinct it is located in. See Part 4 for specific height controls pursuant to relevant zone.

BUILDING DESIGN

To achieve variety in the built form, careful consideration should be given to expression of structure, sunshading devices, balconies, window patterns, roof tops, and overall differentiation of top, middle and base.

Materials should include a mix of compatible materials.

Balcony enclosures will only be permitted where the overall design of the facade is not adversely affected.

Special emphasis should be given to the design of buildings on corner allotments, including consideration of the following:

- how the building addresses its neighbouring buildings, open space, dual frontage; and
- the use of modulated designs to break up the building form.

Buildings should not exceed a total length of 45m. Wall planes should not exceed 30m in length without the roof and wall design being broken.

Buildings are to be designed to make a positive contribution to the street by eliminating blank, featureless walls.

Buildings should allow for some outlook to streets, lanes or other public space areas to increase surveillance and thereby provide for a safer environment.

Visual privacy for adjoining properties and for buildings within the site should, where necessary, be achieved by:

- using windows which are narrow, translucent and obscured;
- ensuring that windows do not face directly onto the windows, balconies or courtyards of adjoining dwellings; and screening opposing windows, balconies and courtyards.

HOUSING MIX

General Controls

- Development is to comply with the controls provided below and in Part 4 of this DCP.
- Lots must have appropriate area and dimensions to enable efficient siting and construction of a dwelling and ancillary buildings, provision of private outdoor space, convenient vehicle access to a public road and adequate parking.
- Tourist development shall be in accordance with the prevailing guidelines.

URBAN DESIGN

- Development shall integrate with existing or proposed development on adjacent sites.
- Development shall provide open space linkages to adjacent environmental protection and / or open space areas.
- Residential development utilising 'gated' street formats is strongly discouraged.
- Subdivision layouts shall establish legible street and open space networks, and shall generally provide for buildings fronting streets to ensure safe, attractive and efficient circulation of pedestrians, cyclists and drivers.
- A landscape plan is required detailing treatment of the streetscape utilising local native species and shall include a three-year maintenance plan. Landscape plans shall be submitted to Council prior to the release of the Subdivision Certificate.
- Landscaping requirements shall not rely on Council land and is to occur on public land only at the request of Council.

URBAN CAPABILITY, SOIL EROSION AND LANDSLIP

Development applications involving subdivision, road construction or remediation work on land sloped by more than 25% shall be accompanied by detailed geotechnical information assessing soil erosion and land slip risks. Land slope categories are mapped in **Map 6 Steep Land**.

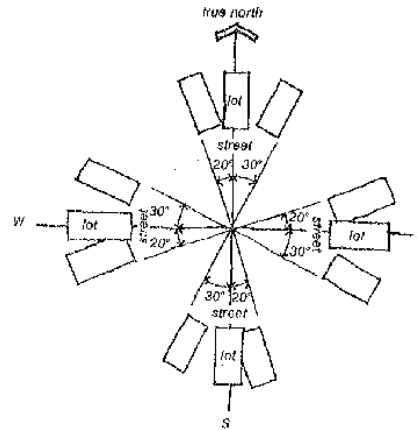
WATER QUALITY

- Development involving earthworks or vegetation removal shall be accompanied by a Sediment and Erosion Control Management Plan in accordance with Council's Policy.
- To ensure the protection of water quality, a 20m buffer is to be provided to all creeks, measured from mean high water mark (as determined by Council).
- These buffer areas are to be rehabilitated and revegetated using endemic species, in accordance with a detailed plan approved by Council, and dedicated as public open space as development proceeds.
- Development applications for subdivision are to be accompanied by water quality modelling to assess the impact of development on water quality compared to the water quality targets identified in Council's Urban Stormwater Management Plan.
- Development is to incorporate Water Sensitive Urban Design in accordance with Council's adopted policy.

ENERGY EFFICIENT DESIGN

- Lots are to be orientated to facilitate the siting of dwellings that will have adequate solar access.
- A minimum of 75 % of lots in any proposed subdivision for single dwelling allotments shall be orientated so that the long axis of the lot is within the range shown in the diagram below.

Residences shall be designed in accordance with Council's Energy Efficiency Information Sheet to meet a 3.5 star efficiency rating.
- Applications for residential subdivision shall include an analysis of cost comparison between solar powered street lighting and main grid supply including a ten-year operating cost for each. Where the total capital cost is equal to or lower for solar-powered supply, Council will require solar-powered street lighting be installed.
- Refer to BASIX website for specifications to meet the requirements of the BASIX Certificate:
www.basix.nsw.gov.au



POTENTIALLY CONTAMINATED LAND

The following requirements apply to any land with a history of banana cultivation and / or potentially contaminated lands. To assist in determining banana cultivation history applicants should check Council's Banana Lands Maps which are available on Council's Website or at the Administration Centre.

- A detailed site assessment report with regard to potential soil and groundwater contamination shall be submitted with development applications for the above land.
- Where Site Assessment Reports, Remedial Action Plan's, or Validation Reports are required as part of development applications, these are to be prepared in accordance with relevant guidelines prepared by the NSW Environment Protection Authority including the following:
 - Guidelines for Assessing Banana Plantation Sites (EPA 1997) and;
 - Guidelines for Consultants Reporting on Contaminated Sites.
- The National Environment and Heritage Protection Council's National Environment Protection (Assessment of Site Contamination) Measure (1999) shall also be considered as part of assessment reports.
- Where the detailed site assessment indicates that a Remedial Action Plan (RAP) is required, this Plan shall accompany all Development Applications.
- These reports are to be prepared by suitably qualified and experienced professionals.
- Where the aggregate area to be remediated is more than three hectares, the development

becomes designated development and requires the preparation of an Environmental Impact Statement.

No remediation is to occur without: Council approval; compliance with the abovementioned guidelines; and compliance with SEPP 55 – Remediation of Land.

General

- Development applications for land being potentially contaminated and / or previously used for banana cultivation shall include as a minimum, consideration of site history and the potential for soil contamination from previous land use.
- Where this consideration identifies soil contamination and / or cannot exclude the possibility of contaminated land, then a detailed site assessment shall be undertaken as above.

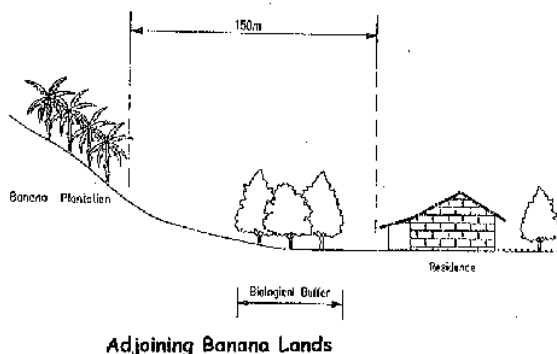
BANANA LANDS BUFFERS

A setback of 150 metres between dwellings and any banana plantation subject to aerial spraying is to be provided unless a negotiated buffer of a lesser distance is otherwise permitted (refer diagram).

A reduced setback is permitted where aerial spraying is reduced or eliminated (i.e. where hand spraying used, where disease resistant species are planted that do not rely on spraying, etc).

A biological buffer of at least 30 metres in width and of mixed species plantings is to be provided as a component of the 150 metre buffer.

The height of the biological buffer planting is to be at least 1.5 times the release height of the aerial spray (approximately 10 to 15 metres high).



CULTURAL HERITAGE

- Any future development has the potential to unearth artifacts. It will be the responsibility of the developer to undertake an Archaeological Assessment.
- An Archaeological Assessment must:
 - be undertaken by person(s) deemed to be suitably qualified by Council and the Local Aboriginal Land Council;
 - comply with the National Parks and Wildlife Act 1974 and the Environmental Planning and Assessment Act 1979; and
 - consider specific 'links', which are important to the Gumbaingirr Aboriginal community.

The requirement of the National Parks and Wildlife Act 1974 must be followed if cultural sites or places are identified on the site. This may necessitate a Section 90 Heritage Impact Permit obtained from the Department of Environment and Climate Change ahead of any site disturbance or destruction.

NATURAL AND HABITAT AREAS

- Development applications shall require assessment of natural and habitat areas.
- Development shall have regard to Council's Koala Plan of Management.
- Where practical, perimeter roads shall be provided adjacent to all environmental protection areas to provide a separation distance between residential development and natural areas. Perimeter roads may be incorporated into Asset Protection Zone (APZ).
- Design of roads that cross key linkages shall incorporate lighting, signage and road narrowing / traffic calming to facilitate safe crossings for koalas.
- Species diversity will be protected where environmental protection zones are implemented and managed to protect prime habitat.
- Indigenous species should be used in street planting and landscaping.
- Habitat linkages will be facilitated in the design and layout of new development.

BUSHFIRE HAZARD

- Establishment of Asset Protection Zones is required to provide a buffer zone between any development and the bushfire hazard. Buffers

must be provided in accordance with the provisions of *Planning for Bushfire Protection 2006*.

- Bushfire Hazard management must comply with the provisions of *Planning for Bushfire Protection 2006*.
- Detailed reporting will be required at the Development Application stage. This will include specific information and assessment in regard to:
 - asset protection zones;
 - building protection;
 - hazard management;
 - evacuation safety;
 - availability of fire fighting services;
 - access for firefighting operations;
 - water supplies; and
 - communications.
- All hydrant locations shall be provided with hardstand areas for positioning of fire appliances filling their tanks.

Subdivision development shall ensure the provision of fire hydrants and perimeter fire roads or trails.

NOISE AND AIR QUALITY

- Applications for subdivision within 300m of the current Pacific Highway, the Preferred Route for the Coffs Harbour Bypass, within 100m of the North Coast Railway Line, and / or land adjoining existing industrial uses will be required to be accompanied by an acoustic report. This report should identify measures (mounding, acoustic barriers, building design, building materials, etc) to ensure future residents are not subjected to unacceptable noise levels.
- Assessment is to be undertaken by an appropriately qualified person, having regard to the following guidelines: *NSW Industrial Noise Policy (EPA 2000)* and the *NSW Environmental Criteria for Road Traffic Noise (EPA 1999)*.
- Noise reduction may require judicious design in terms of location factors, internal layouts, building materials and construction.
- Careful planning and consideration of potential noise issues at the detailed design stage can assist in achieving internal noise goals across the majority of the site. The following design and construction measures are suggested:

- locate dwellings on each allotment as far as possible from the noise source;
- minimise the size and number of windows facing the noise source;
- locate the noise insensitive areas such as the kitchen, storage areas, and laundry towards the noise source;
- use construction techniques that focus on sealing gaps around windows, doors and ceiling spaces etc;
- use thick or laminated glass on windows toward the noise source;
- use solid core doors and appropriate door seals; and
- replace traditional roof design with eaves by a flat roof with parapets (recommended for apartment buildings).

ROAD DESIGN AND ACCESS CONTROL

- Development shall be designed in accordance with the Traffic and Transport Strategy detailed in Part 2.
- Road design and construction works are to conform to Council's Development Design Specification.
- Local native street trees are to be planted and protected with tree guards and maintained for a period of three years.
- No access will be permitted directly onto the Pacific Highway.
- Threshold and pavement treatments are to be provided throughout the subdivision to create an attractive streetscape.

Roads are to be constructed in accordance with the standards in Table 3.1:

Table 3.1 Road Construction

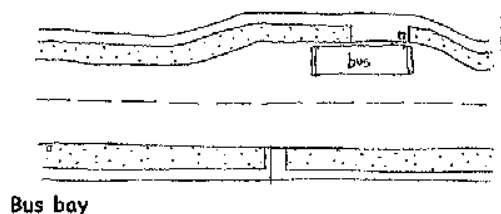
	Collector Road	Local	Minor Road
Road Reserve Width	20 m	15-16 m	13.5 – 15 m
Carriageway Width	9-11 m	7-8 m	5.5 –7 m
Verge	4.5 – 5.5 m	2 x 4.0 m	2 x 4.0 m
Minimum Design Speed	50 km/h	40 km/h	25 km/h

4.5	9-11	4.5	Collector
4.0	7-8	4.0	Local
4.0	5.5-7	4.0	Minor Road (cul-de-sac)

Road Design for Bus Access

Development is to provide for:

- the safe and efficient movement of buses through the subdivisions;
- appropriate road widths on roads likely to form part of a bus route;
- linking bus routes and bus stops to the pedestrian network;
- bus turning area and bus shelters;
- road widths and bus bays are to be provided in the design of subdivision to meet the requirements shown in the diagram; and
- bus shelters are to be provided through developer contributions.



PEDESTRIAN AND CYCLEWAY PATHS

- Pedestrian paths and cycleways are to be constructed by the developer carrying out subdivision, except where the paths / cycleways are within identified future open space areas, where construction will be carried out as part of the developer contribution plan.
- Playground equipment, pedestrian paths and cycleways are to be provided as shown in **Map 1**.
- The developer is to fund provision of footpaths in accordance with Council's requirements and the North Coffs Contribution Plan.
- Playground equipment is to be funded by developer contributions.

SITE ANALYSIS

Site analysis is required to identify opportunities and constraints for building design.

A site analysis plan (at scale 1:200) is to be provided with any application for development of this land. The site analysis shall include:

- site dimensions (length, width);
- spot levels or contours;
- north point;
- natural drainage;
- any contaminated soils or filled areas;
- services (easements, utilities);
- existing trees (height, spread, species);
- views to and from site;
- prevailing winds; and
- surrounding buildings.

DEVELOPMENT PRECINCTS

As discussed in **Part 2**, the areas within the site suitable for residential development have been grouped into four discrete precincts. These are R1 and R2 (deferred), R3 and T1 and R4. There are also areas dedicated as Community Open Space and Environmental Conservation.

Further development controls for each precinct are provided in Part 4 of this DCP.

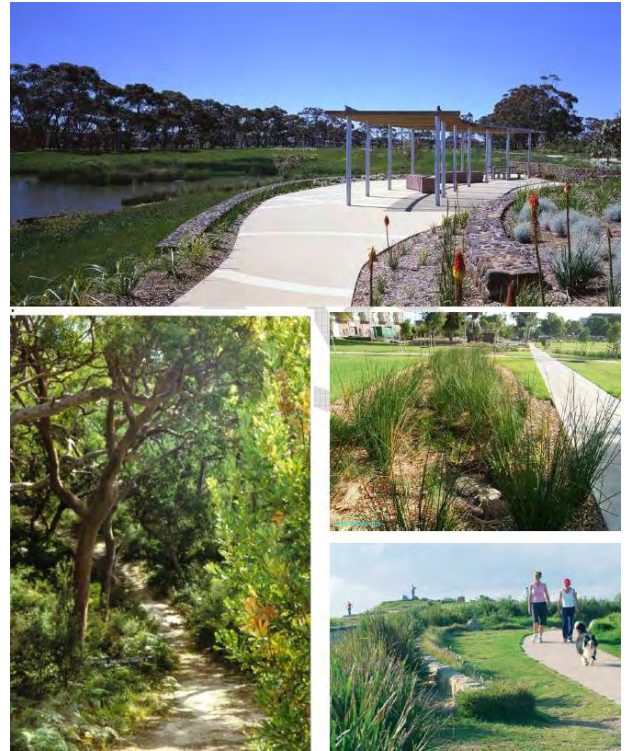
COMMUNITY OPEN SPACE AND ENVIRONMENTAL CONSERVATION AREAS

In order to maintain and enhance the visual, environmental and water management values of the existing setting, a large section of the North Coffs Release Area should be excluded from development. These excluded areas should be retained as a mixture of community open space, environmental protection and rural land which should surround and infiltrate development areas, dominating their visual character and creating attractive settings for residents while protecting the site's natural and aesthetic amenity for the benefit of the wider community.

Included within the community open spaces should be the small pockets of existing remnant native vegetation that occur across the site. These areas should be conserved, enhanced and expanded to connect existing fragments where possible.

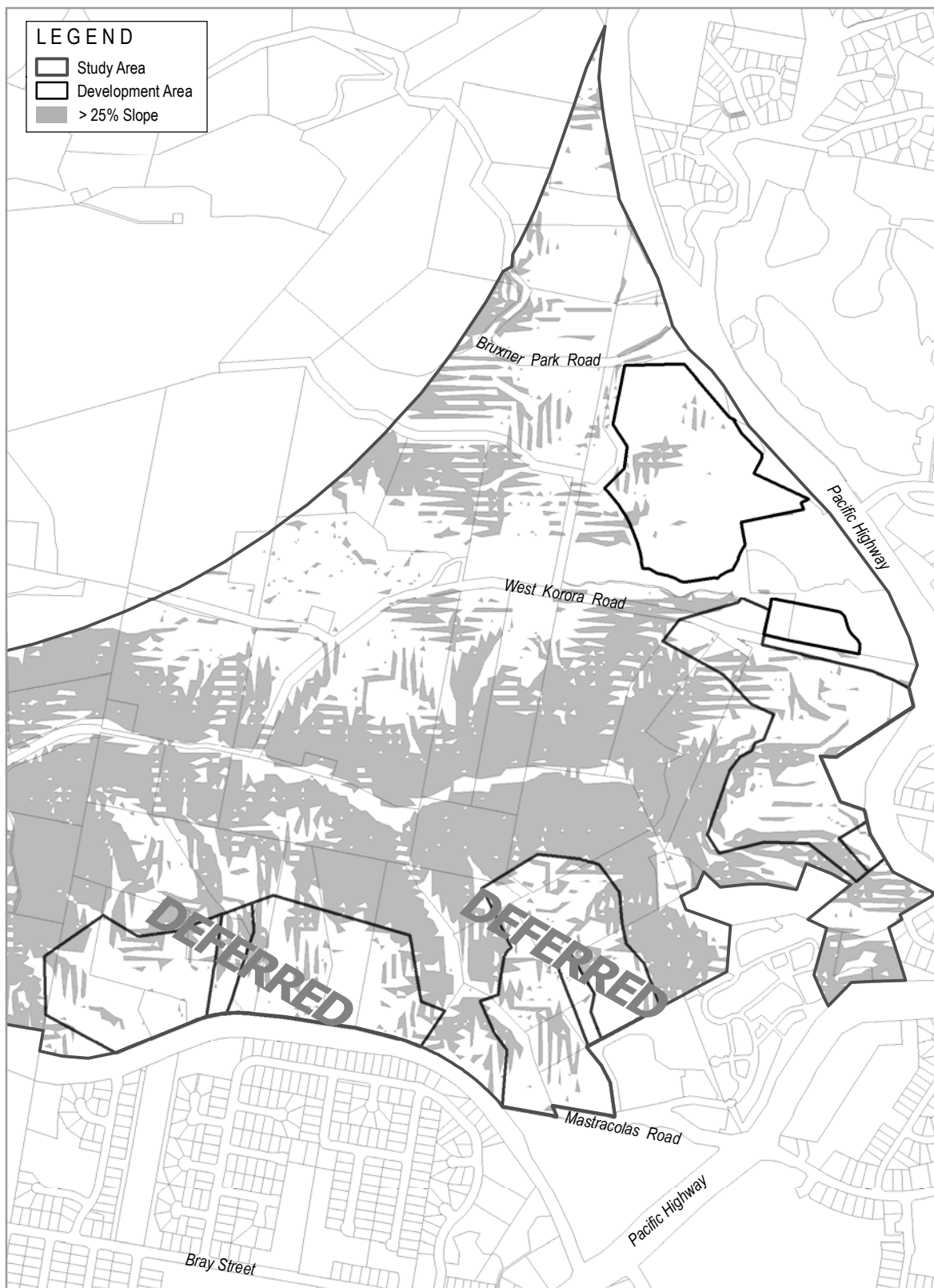
Regeneration of the riparian vegetation is also encouraged along the existing watercourses to overcome the effects of past grazing practices. This should aim to provide new habitat for native fauna and to enhance the value of existing natural elements.

The community open spaces should incorporate a network of pedestrian and bicycle paths to enhance recreational opportunities of the residents and to create strong connections between the various precincts. The paths should be designed and located to optimise the diversity of visual experiences offered by the site and to incorporate rest areas and interpretive signs to enhance the recreational experience for users.



Indicative Community Open Space Areas

MAP 6 - STEEP LAND



North Coffs
Development Control Plan

Map 6 - Steep Land

N
1:12,500

PART 4 – GENERAL DEVELOPMENT CONTROLS

RESIDENTIAL LOW DENSITY

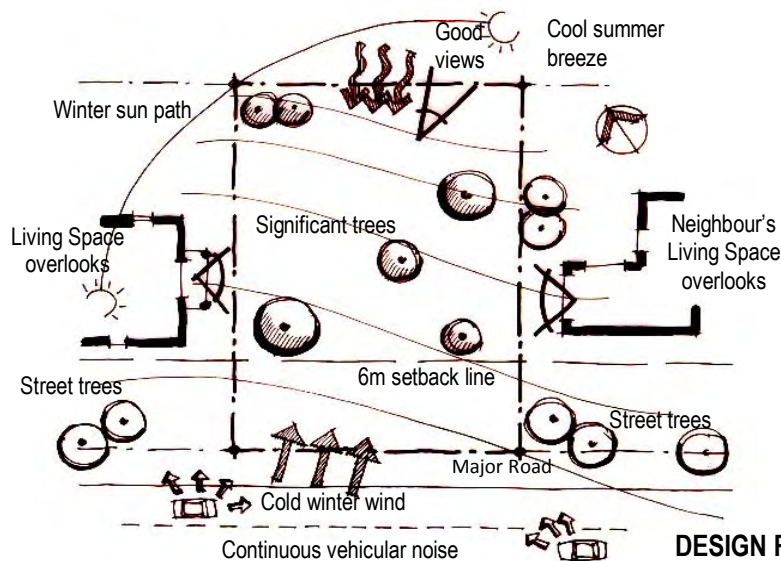
This section of the DCP applies to the R3 and R4 precincts, indicated on Map 1 Masterplan.

OBJECTIVES

The Residential Low Density controls seek to:

- encourage innovative housing that is pleasant to live in, relates to the desired future neighbourhood character, is responsive to the site and is environmentally sensitive; and
- improve the quality and choice of housing and residential environments to suit the diversity of people's needs and to meet community expectations about health, safety and amenity.

SITE ANALYSIS



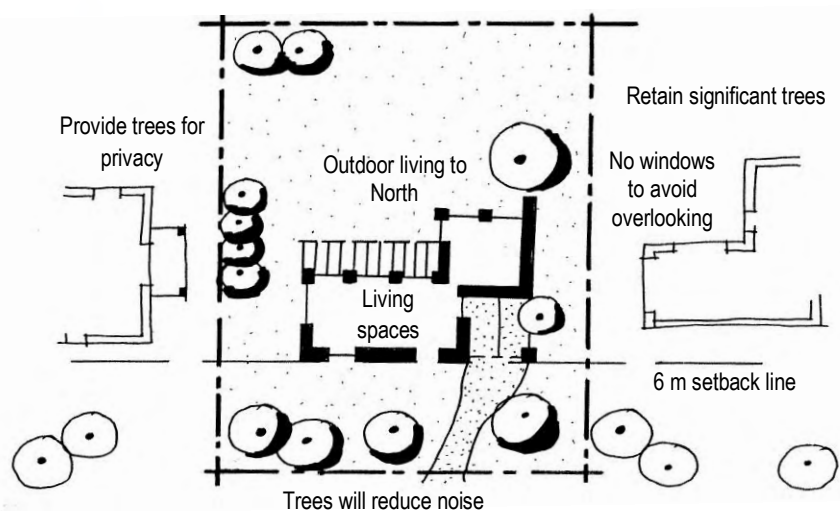
SITE ANALYSIS

Site analysis is required to identify opportunities and constraints for building.

A site analysis plan (at scale 1:200) is to include:

- site dimensions (length, width);
- spot levels or contours;
- north point;
- existing site drainage;
- any contaminated soils or filled areas;
- services (easements, utilities);
- existing trees (height, spread, species);
- views to and from site;
- prevailing winds; and
- surrounding development (where three or more dwellings are proposed).

DESIGN RESPONSE



CONTROLS

Building Height

The height of a dwelling house or the alterations and additions to an existing dwelling house must not be more than 8.5m above ground level (existing).

Note: building height (or height of building), at any point of a building, means the vertical distance between that point at ground level (existing) and the highest point of the building immediately above that point, including plant and lift overruns, but excluding communication devices, antennae, satellite dishes, masts, flagpoles, chimneys, flues and the like.

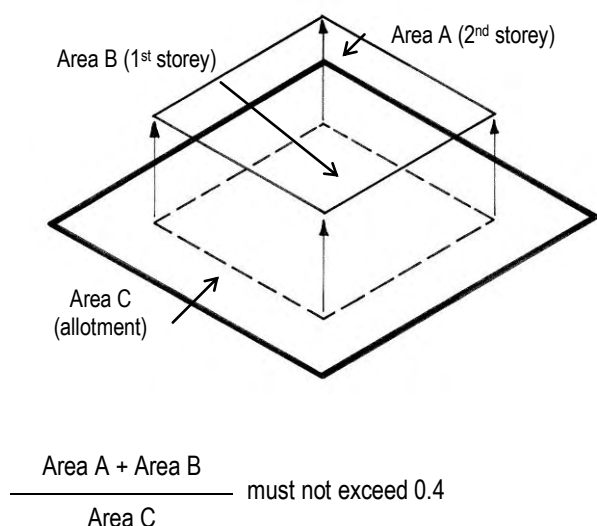
Lot Dimensions

Density is not to exceed one dwelling per 400m², not including the area of any access handle or internal driveway. For lots fronting a cul-de-sac head the density is one dwelling per 500m².

All developments should provide a mix of dwelling sizes where possible.

A minimum area of 185m² is to be provided for each dwelling for landscaping.

For dual occupancy development, the floor area of the proposed dwellings is not to exceed a floor space ratio of 0.4:1. The following diagram indicates this.



Note:

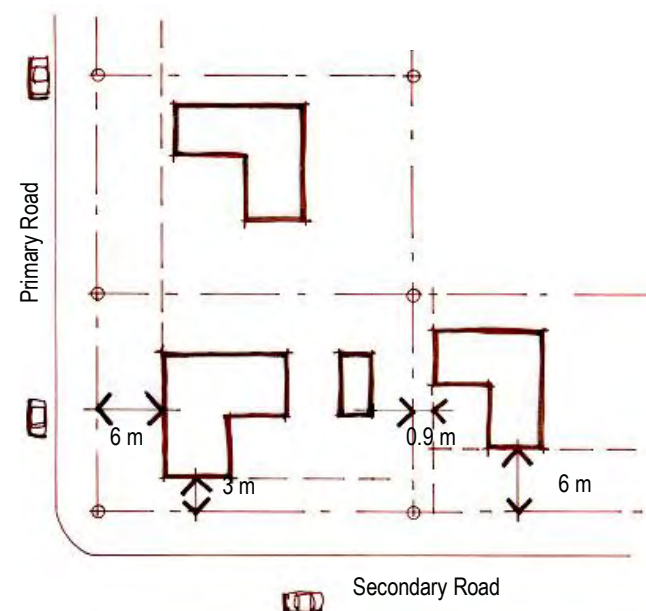
- The floor area is the sum of the areas of each floor of both dwellings excluding verandahs, all parking areas, storage sheds and the like. The floor area is to be measured from the outer face of the external enclosing walls.
- The floor space ratio is the floor area of the proposed dwellings, divided by the area of the existing lot.

Note:

Landscaping is that part of the lot not used for driveways, car spaces or buildings.

SETBACKS

- Buildings are to be generally setback a minimum of 6m from the front boundary. On corner lots buildings can be setback 3m from the secondary street boundary.
- Buildings are to be setback 20m from creeks and major watercourses.
- Side and rear setbacks are generally 900mm from walls and 675mm to outer edge of roof gutter and eaves.
- Buildings can be built to side and rear boundaries (zero setbacks) where:
 - the building has maximum boundary wall height of 3m, unless matching an existing or

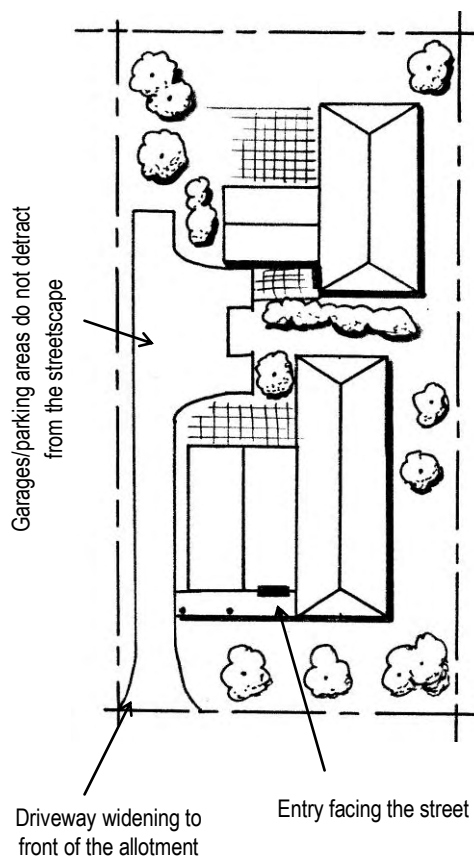


simultaneously constructed wall;

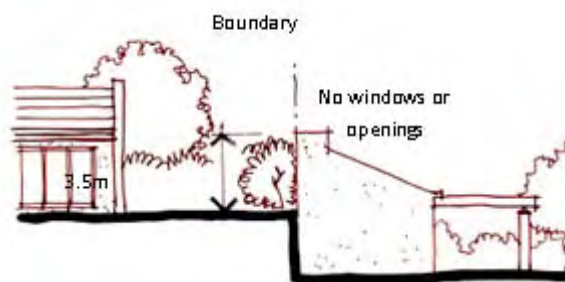
- satisfactory legal arrangements for maintenance of boundary walls are in place;
- there is no adverse impact upon the amenity of the adjoining properties;

- there is no interruption to overland drainage paths;
 - generally, should only occur on the southern boundary;
 - there are no openings in the boundary wall; and
 - the wall is of fire rated masonry construction.
- The design of buildings is interesting and livable by incorporating some or all of these elements:
 - decks;
 - pergolas;
 - verandahs;
 - eaves;
 - lattice; and
 - climate control windows (i.e. louvres or small windows, selected glass).
 - Use pier or pole construction on slopes in excess of 20%. A maximum of 1m cut or fill is allowed outside the buildings external walls.
 - Buildings are to be designed to enhance the street, by eliminating blank, featureless walls, which detract from the appearance of the street.
 - The buffering provisions contained in the NSW Department of Primary Industries document *Living and Working in Rural Areas* also apply.

DESIRABLE DESIGN FOR DUAL OCCUPANCY



ZERO SETBACK



- Buildings should allow for some outlook to streets, lanes or other public space areas to increase surveillance and thereby provide for a safer environment.
- Minimise direct overlooking of living areas and private open space of other dwellings.
- Building design should provide an appropriate scale to the streetscape.
- Garages and parking structures are to be sited and designed so as not to dominate the street frontage.
- Where dual occupancy development is proposed and both dwellings will have frontage to the street, the dwellings are not to be mirror reversed.



Mirror reversing: unacceptable design solution

Source www.designcommunity.com

- Fencing is not to be located along a boundary adjoining public land unless it is no greater than 1.5m high, and includes either a setback for landscaping purposes, or recesses for this same purpose.

Note:

Council is not bound to enforce S88B instruments in private matters such as density and developer preferred building materials.

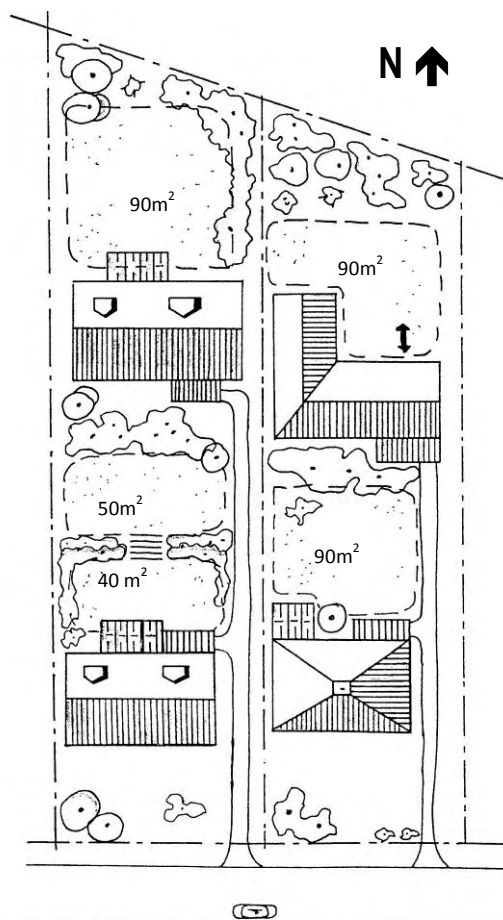
SOLAR ACCESS

- Buildings should be designed to allow at least two hours of sunshine upon the living areas of adjacent dwellings and open space areas between 9.00 am and 3.00 pm on 22 June.
- Where the possibility of overshadowing may occur, shadow diagrams are to be submitted to illustrate the shadows cast by the proposed building at 9.00 am, 12.00 noon and 3.00 pm on 22 June.

PRIVATE OPEN SPACE

Each dwelling is to have private open space with direct connection to indoor living areas through sliding glass doors or other similar openings. This space should be oriented to provide for maximum year round use.

- A minimum of 90m² is to be provided for each dwelling, preferably provided in one area. Consideration may be given to the division of this area into two areas, with the smallest portion being no less than 40m².
- Each open space area is to have a minimum dimension of 4m and a slope not greater than 1 in 8.
- Private open space (including swimming pool areas) is not to be located at the front of a development adjoining public road, unless details of satisfactory fencing are included with the proposal.
- Dwellings are not to unreasonably compromise the private open space of nearby dwellings.



VEHICLE ACCESS AND PARKING

Parking

- Off street car parking areas are to be provided in accordance with the Off Street Car Parking section of this plan.
- Car parking is to be provided behind the front setback of the building.
- Car parking areas should be incorporated into the building or provided at, or behind, the front setback of the building.
- Where more than one space is required one parking space is allowed between the dwelling and the front boundary.
- The minimum dimensions of car parking spaces should be 2.4m x 5.5m.

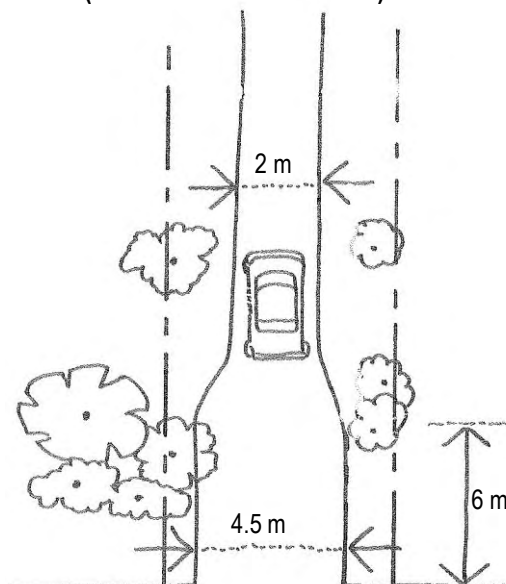
Driveways

- To reduce the impacts of stormwater runoff, improve visual amenity, and maintain on-street car parking driveways should be minimised.
- A driveway, which serves a maximum of three dwellings, is to have a minimum paved width of 2m.
- A shared driveway, which serves three or more dwellings, is to have a minimum paved width of 4.5m at the street, continuing at this width to a depth of 6m.
- Long driveways may require 'passing points' (particularly on busy roads).
- Provision is to be made for vehicles to enter and leave the site in a forward direction, where the site is:
 - steep;
 - fronts a busy road;
 - has three or more dwellings on it;
 - subject to high pedestrian use; or
 - where driveways are more than 30m in length.

Turning areas are to be designed to allow the 85% Design Car Turning Path.

- Driveways should have gradients less than 20% and the driveway grade should not change by more than 11% for every 1.4m of driveway.

DRIVEWAYS (3 OR MORE DWELLINGS)



Note:

For minimum lot frontages and width of battleaxe handles also refer Council's Subdivision DCP.

DEVELOPER CONTRIBUTIONS

Developer contributions are payable for any development creating two or more dwellings on one lot or development that increases demand of facilities and services. Developer contributions must be paid in accordance with the North Coffs Developer Contribution Plan

Note:

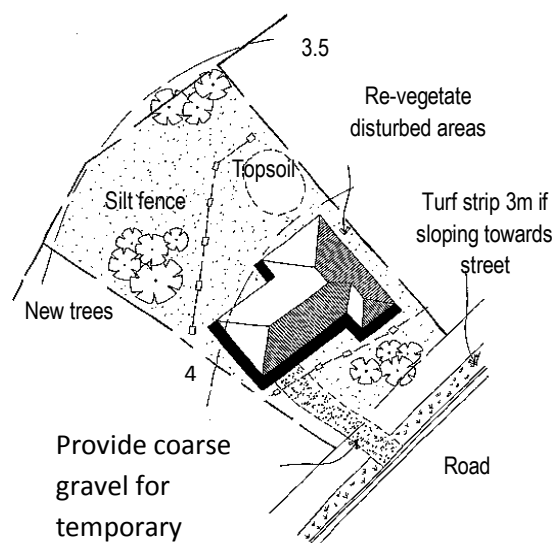
Developer contributions are a monetary payment to fund increased demand for public facilities generated by the development.

EROSION AND SEDIMENT CONTROL

For proposals for three or more dwellings an Erosion and Sediment Control Plan is required to be submitted to and approved by Council prior to the release of the construction certificate; refer to Council's "Erosion and Sediment Control on Building and Development Sites – Policy and Code of Practice".

For proposals for two or less dwellings the following is required:

- three strips of turf parallel to, and against, the kerb;
- coarse gravel to define a single construction access no more than 3m wide;
- install sediment fence:
 - along the road frontage immediately upslope of the turf strips or around the low side of the area of construction if the site slopes away from the road;
 - around the low side of stockpiles; and
 - with the ends of the fences turned upslope;
- all stockpiles of topsoil, sand, aggregate, spoil, vegetation or other material capable of being moved by running water shall be stored clear of any drainage lines, easements or natural watercourses, footpath, kerb or road surface;
- before roofing material is laid, temporary or permanent guttering and downpipes shall be installed and connected to an approved stormwater disposal system; and
- all disturbed areas shall be rendered erosion resistant by revegetation or landscaping within four weeks of building activities being completed or suspended.



SERVICES

Water Meters

- A separate water meter is to be provided for each dwelling and is to be readily accessible to Council's meter reader.

Stormwater

- All stormwater is to be directed to the street drainage system, or to an interallotment drainage easement where available. Surface water is not to be directed to neighbouring properties. Stormwater to kerb connections are to be via kerb adapter units. House fencing should not obstruct overland flows of water. House floor levels must be at least 300mm above finished ground level.

Note:

Interallotment drainage via easements may be required.

RESIDENTIAL MEDIUM DENSITY

This section of the DCP applies to the R1 and R2 precincts, indicated on Map 1 Masterplan. It also applies to non-tourist accommodation development in the R3 precinct, indicated on Map 1 Masterplan.

OBJECTIVES

The Residential Medium Density controls seek to:

- encourage innovative housing that is pleasant to live in, relates to the desired future neighbourhood character, is responsive to the site and is environmentally sensitive; and
- improve the quality and choice of housing and residential environments to suit the diversity of people's needs and to meet community expectations about health, safety and amenity.

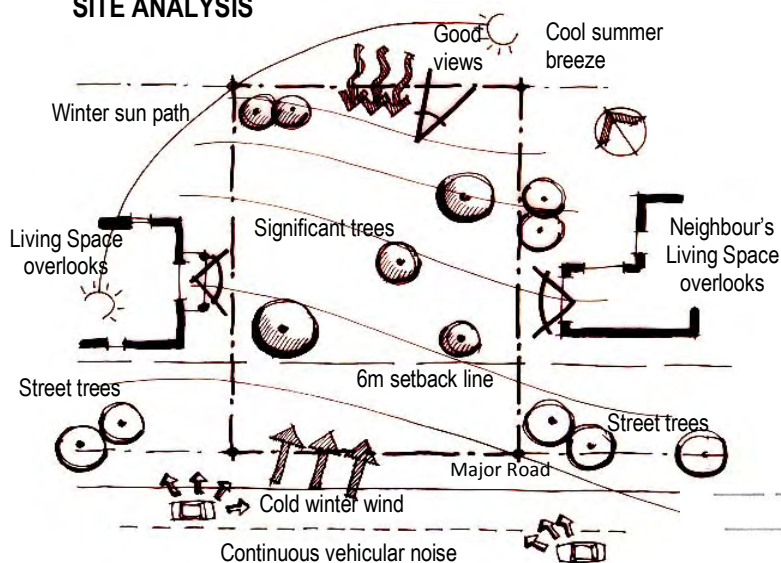
SITE ANALYSIS

Site analysis is required to identify opportunities and constraints for building design.

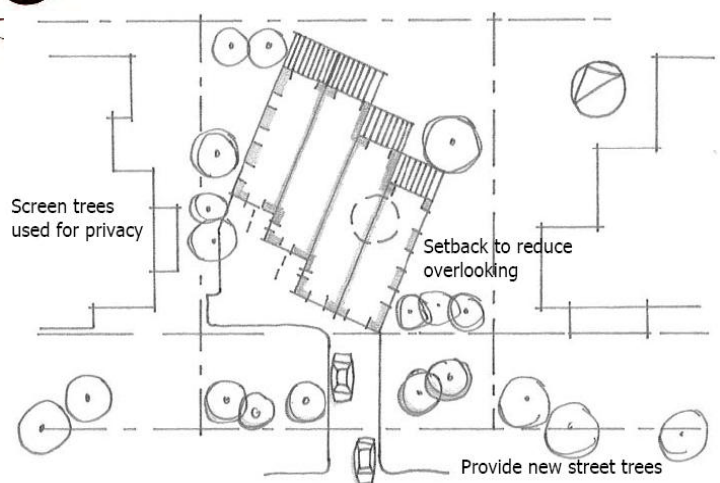
A site analysis plan (at scale 1:200) is to include:

- site dimensions (length, width);
- spot levels or contours;
- north point;
- natural drainage;
- any contaminated soils or filled areas;
- services (easements, utilities);
- existing trees (height, spread, species);
- views to and from site;
- prevailing winds; and
- surrounding buildings.

SITE ANALYSIS



DESIGN RESPONSE



DEVELOPMENT CONTROLS

Building Height

Buildings are not to exceed 8.5 metres in height (generally no more than two storeys). Underground car parking should not protrude more than 1m above natural ground level.

Note: building height (or height of building), at any point of a building, means the vertical distance between that point at ground level (existing) and the highest point of the building immediately above that point, including plant and lift overruns, but excluding communication devices, antennae, satellite dishes, masts, flagpoles, chimneys, flues and the like.

Lot Dimensions/Density

Density is not to exceed one dwelling per 200m² of site area. A minimum area of 88m² is to be provided for each dwelling for landscaping.

All developments should provide a mix of dwelling sizes where possible.

SETBACKS

The minimum permissible setback distances are provided in **Table 4.1** below.

Table 4.1 R1and R2 Minimum Setbacks

Boundary	Distance
Front	6m
Second street frontage (where a corner lot)	3m
Side and Rear	900mm from walls and 675mm to outer edge of roof gutter and eaves.
Zero Setbacks	
Buildings can be built to side and rear boundaries (zero setbacks) where:	
<ul style="list-style-type: none">the building has maximum boundary wall height of 3m, unless matching an existing or simultaneously constructed wall;satisfactory legal arrangements for maintenance of boundary walls are in place;there is no adverse impact upon the amenity of the adjoining properties;there is no interruption to overland drainage paths;should only occur on southern boundary;there are no openings in the boundary wall; andthe wall is of fire rated masonry construction.	

The buffering provisions contained in the NSW Department of Primary Industries document *Living and Working in Rural Areas* also apply.

SOLAR ACCESS

- Buildings should be designed to allow at least two hours of sunshine upon the living areas of adjacent dwellings and open space areas between 9.00 am and 3.00 pm on 22 June.
- Where the possibility of overshadowing may occur, shadow diagrams are to be submitted to illustrate the shadows cast by the proposed building at 9.00 am, 12.00 noon and 3.00 pm on 22 June.

PRIVATE OPEN SPACE

The private open space requirements for dwellings are provided in **Table 4.2** below.

Table 4.2 R1and R2 Private Open Space

Medium Density Area Provisions
Each dwelling is to have 32m ² of private open space with direct connection to indoor living areas.
Private open space areas are to have a minimum dimension of 3m and a slope not greater than 1 in 8.
Private open space (including swimming pools) is not to be located at the front of a development adjoining public road, unless details of satisfactory fencing are included with the proposal.

LANDSCAPING

A landscape plan must be prepared by a qualified landscape architect or designer for new development showing the location of existing trees, proposed landscaping and trees to be removed or retained.

Landscaping should be provided in front and side setback areas and other areas of the site to improve the streetscape, soften the appearance of buildings and paved areas and to provide shade, shelter and visual screening.

Landscaping should include species that will grow to a height consistent with the building height.

The landscaping should include one tree (to building height), one tall shrub (min. 4m tall) and six shrubs (min. 1m tall) per unit.

Note:

Refer to the Landscaping Information Sheet for guidelines on landscape planning and species selection.

ACCESS AND VEHICLE PARKING

Off street car parking areas are to be provided in accordance with the Off Street Car Parking section of this plan.

Resident Parking

Garages and parking structures are sited and designed so as not to dominate the street frontage.

The minimum internal dimensions of enclosed garages is 3m x 6m. Minimum headroom in undercover parking is 2.1m.

Garage doors and parking spaces can be widened if maneuvering areas are limited.

Car parking areas are to be incorporated into the building or provided at, or behind, the front setback of the building.

Where more than one space is required one parking space is allowed between the dwelling and the front boundary.

Parking spaces shall be designed in accordance with Australian Standard 2890.1 and 2890.2.

Visitor / Overflow Parking

Visitor / overflow parking is to be provided at a rate of one space per every five dwellings or part thereof.

Visitor / overflow parking is to be behind the front setback and freely accessible at all times.

Visitor / overflow car parking where proposed must be clearly detailed in the development documentation.

For additional information on car parking provisions, please refer to the Off Street Car Parking DCP.

Driveways

To reduce the impacts of stormwater runoff, improve visual amenity, and maintain on-street car parking driveways is to be minimised.

A driveway, which serves a maximum of three dwellings, is to have a minimum paved width of 2m.

A shared driveway, which serves three or more dwellings, is to have a minimum paved width of 4.5m at the street, continuing at this width to a depth of 6m.

Long driveways may require 'passing points' (particularly on busy roads).

Provision is to be made for vehicles to enter and leave the site in a forward direction, where the site is:

- steep;
- fronts a busy road;
- has three or more dwellings on it;
- subject to high pedestrian use; or
- where driveways are more than 30m in length.

Turning areas are to be designed to allow the 85 % Design Car Turning Path.

Driveways should have gradients less than 20 % and the driveway grade should not change by more than 10 % for every 1.4m of driveway (minimum lengths). Driveway design must comply with Australian Standard 2890.1.

DEVELOPER CONTRIBUTIONS

Developer contributions are payable for any development creating two or more dwellings on one lot.

Note:

Developer contributions are a monetary payment to fund increased demand for public facilities generated by the development.

EROSION AND SEDIMENT CONTROL

For proposals for three or more dwellings an Erosion and Sediment Control Plan is required to be submitted to and approved by Council prior to the release of the construction certificate; refer "Erosion and Sediment Control on Building and Development Sites – Policy and Code of Practice".

For proposals for two or less dwellings the following is required:

- three strips of turf parallel to, and against, the kerb;
- coarse gravel to define a single construction access no more than 3m wide;
- install sediment fence:
 - along the road frontage immediately upslope of the turf strips or around the low side of the area of construction if the site slopes away from the road;
 - around the low side of stockpiles; and
 - with the ends of the fences turned upslope;
- all stockpiles of topsoil, sand, aggregate, spoil, vegetation or other material capable of being moved by running water shall be stored clear of any

drainage lines, easements or natural watercourses, footpath, kerb or road surface;

- before roofing material is laid, temporary or permanent guttering and downpipes shall be installed and connected to an approved stormwater disposal system; and
- all disturbed areas shall be rendered erosion resistant by revegetation or landscaping within four weeks of building activities being completed or suspended.

SERVICES

Water Meters

A separate water meter is to be provided for each dwelling and is to be readily accessible to Council's meter reader.

Stormwater

All stormwater is to be directed to the street drainage system, or to an interallotment drainage easement where available. Surface water is not to be directed to neighbouring properties. Stormwater to kerb connections are to be via kerb adapter units. House fencing should not obstruct overland flows of water. House floor levels must be at least 300mm above finished ground level.

Note:

Interallotment drainage via easements may be required.

RESIDENTIAL TOURIST ACCOMMODATION

This section of the DCP applies tourist accommodation when carried out in the R3 precinct indicated on Map 1 Masterplan.

OBJECTIVES

The Residential Tourist Accommodation controls seek to:

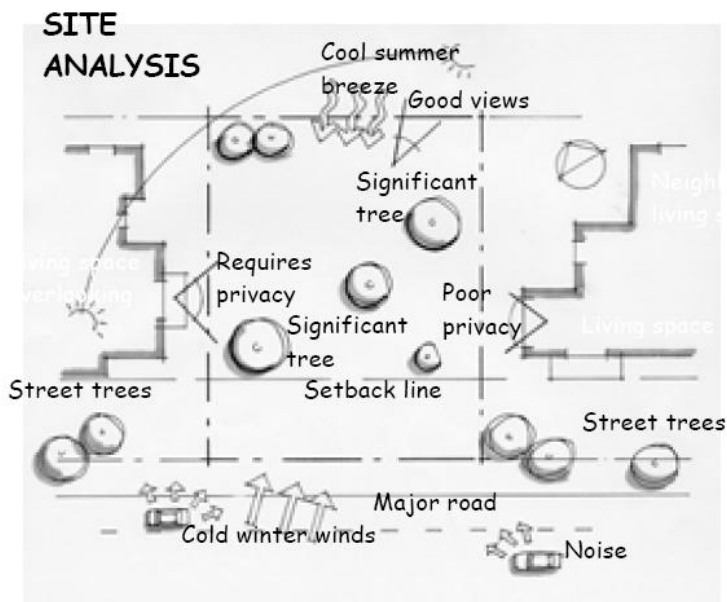
- encourage tourism development;
- ensure that the scale and intensity of residential and tourist development is appropriate to the environmental characteristics of the land and locality; and
- promote ecologically sustainable development.

SITE ANALYSIS

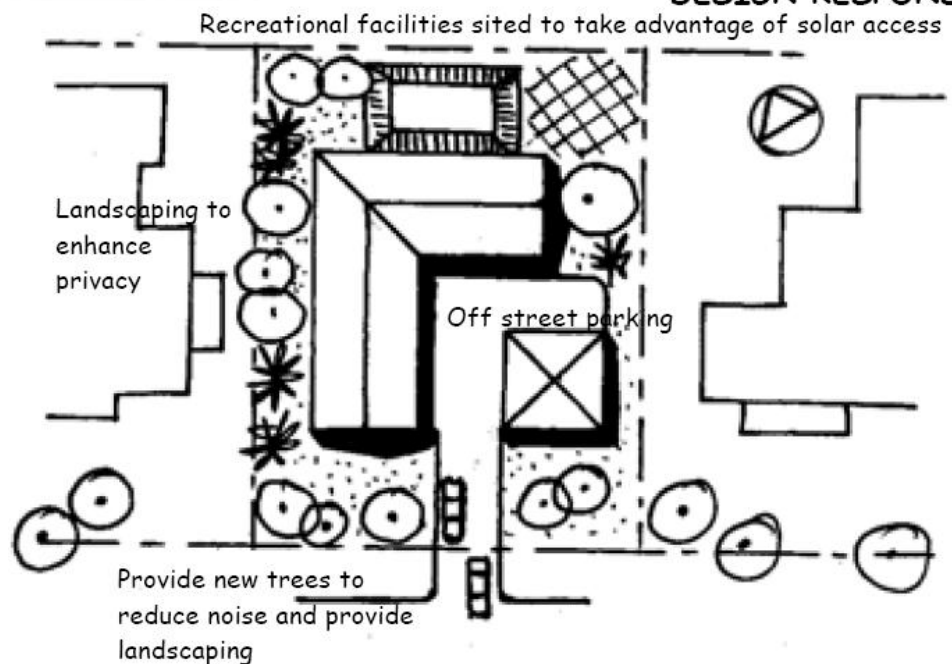
Site analysis is required to identify opportunities and constraints for building design.

A site analysis plan (at scale 1:200) is to include:

- site dimensions (length, width);
- spot levels or contours;
- north point;
- natural drainage;
- any contaminated soils or filled areas;
- services (easements, utilities);
- existing trees (height, spread, species);
- views to and from site;
- prevailing winds; and
- surrounding buildings.



DESIGN RESPONSE



DEVELOPMENT CONTROLS

Building Height

Buildings are not to exceed 17.5 metres in height. Underground car parking should not protrude more than 1m above natural ground level.

Note: building height (or height of building), at any point of a building, means the vertical distance between that point at ground level (existing) and the highest point of the building immediately above that point, including plant and lift overruns, but excluding communication devices, antennae, satellite dishes, masts, flagpoles, chimneys, flues and the like.

Lot Dimensions / Density

Density is not to exceed one dwelling per 150 m², not including the area of any access handle or internal driveway.

For lots fronting a cul-de-sac head the density is one dwelling per 500m².

All developments should provide a mix of dwelling sizes where possible.

SETBACKS

The minimum permissible setback distances are provided in **Table 4.3** below.

Table 4.3 R1 Minimum Setbacks

Boundary	Distance
Front	6m
Second street frontage (where a corner lot)	Assessed on merit
Side and Rear	Assessed on merit.

The buffering provisions contained in the NSW Department of Primary Industries document *Living and Working in Rural Areas* also apply.

SOLAR ACCESS

- Buildings should be designed to allow at least two hours of sunshine upon the living areas of adjacent dwellings and open space areas between 9.00 am and 3.00 pm on 22 June.
- Where the possibility of overshadowing may occur, shadow diagrams are to be submitted to illustrate the shadows cast by the proposed building at 9.00 am, 12.00 noon and 3.00 pm on 22 June.

PRIVATE OPEN SPACE

Private open space requirements for tourist accommodation area provided in **Table 4.4** below.

Table 4.4 Private Open Space

Accommodation Size	Requirement
Small (<55m ² GFA)	8m ²
Medium (55m ² – 84m ² GFA)	10m ²
Large (85m ² – 125m ² GFA)	12m ²
Extra large (>125m ² GFA)	16m ²

Private open space is to have direct connection to indoor living areas and is to have a minimum width of 2m.

Private open space (including swimming pools) is not to be located at the front of a development adjoining public road, unless details of satisfactory fencing are included with the proposal.

LANDSCAPING

A landscape plan must be prepared by a qualified landscape architect or designer for new development showing the location of existing trees, proposed landscaping and trees to be removed or retained.

Landscaping should be provided in front and side setback areas and other areas of the site to improve the streetscape, soften the appearance of buildings and paved areas and to provide shade, shelter and visual screening.

Landscaping should include species that will grow to a height consistent with the building height.

The landscaping should include one tree (to building height), one tall shrub (min. 4m tall) and six shrubs (min. 1m tall) per unit.

Note:

Refer to the Landscaping Information Sheet for guidelines on landscape planning and species selection.

ACCESS AND VEHICLE PARKING

Off street car parking areas are to be provided in accordance with the Off Street Car Parking section of this plan.

Resident Parking

Garages and parking structures are sited and designed so as not to dominate the street frontage.

The minimum internal dimensions of enclosed garages is 3m x 6m. Minimum headroom in undercover parking is 2.1m.

Garage doors and parking spaces can be widened if maneuvering areas are limited.

Car parking areas are to be incorporated into the building or provided at, or behind, the front setback of the building.

Where more than one space is required one parking space is allowed between the dwelling and the front boundary.

Parking spaces shall be designed in accordance with Australian Standard 2890.1 and 2890.2.

Visitor / Overflow Parking

Visitor / overflow parking is to be provided at a rate of one space per every five dwellings or part thereof.

Visitor / overflow parking is to be behind the front setback and freely accessible at all times.

Visitor / overflow car parking where proposed must be clearly detailed in the development documentation.

For additional information on car parking provisions, please refer to the Off Street Car Parking section of this plan.

Driveways

To reduce the impacts of stormwater runoff, improve visual amenity, and maintain on-street car parking driveways is to be minimised.

A driveway, which serves a maximum of three dwellings, is to have a minimum paved width of 2m.

A shared driveway, which serves three or more dwellings, is to have a minimum paved width of 4.5m at the street, continuing at this width to a depth of 6m.

Long driveways may require 'passing points' (particularly on busy roads).

Provision is to be made for vehicles to enter and leave the site in a forward direction, where the site is:

- steep;
- fronts a busy road;
- has three or more dwellings on it;
- subject to high pedestrian use; or
- where driveways are more than 30m in length.

Turning areas are to be designed to allow the 85 % Design Car Turning Path.

Driveways should have gradients less than 20 % and the driveway grade should not change by more than 10 % for every 1.4m of driveway (minimum lengths). Driveway design must comply with Australian Standard 2890.1.

DEVELOPER CONTRIBUTIONS

Developer contributions are payable for any development creating two or more dwellings on one lot.

Note:

Developer contributions are a monetary payment to fund increased demand for public facilities generated by the development.

EROSION AND SEDIMENT CONTROL

For proposals for three or more dwellings an Erosion and Sediment Control Plan is required to be submitted to and approved by Council prior to the release of the construction certificate; refer "Erosion and Sediment Control on Building and Development Sites – Policy and Code of Practice".

For proposals for two or less dwellings the following is required:

- three strips of turf parallel to, and against, the kerb;
- coarse gravel to define a single construction access no more than 3m wide;
- install sediment fence:
 - along the road frontage immediately upslope of the turf strips or around the low side of the area of construction if the site slopes away from the road;
 - around the low side of stockpiles; and
 - with the ends of the fences turned upslope;
- all stockpiles of topsoil, sand, aggregate, spoil, vegetation or other material capable of being moved by running water shall be stored clear of any drainage lines, easements or natural watercourses, footpath, kerb or road surface;
- before roofing material is laid, temporary or permanent guttering and downpipes shall be installed and connected to an approved stormwater disposal system; and
- all disturbed areas shall be rendered erosion resistant by revegetation or landscaping within four weeks of building activities being completed or suspended.

SERVICES

Water Meters

A separate water meter is to be provided for each dwelling and is to be readily accessible to Council's meter reader.

Stormwater

All stormwater is to be directed to the street drainage system, or to an interallotment drainage easement where available. Surface water is not to be directed to neighbouring properties. Stormwater to kerb connections are to be via kerb adapter units. House fencing should not obstruct overland flows of water. House floor levels must be at least 300mm above finished ground level.

Note:

Interallotment drainage via easements may be required.

BUSINESS LANDS

This section of the DCP applies to the part of T1 precinct indicated on Map 1 Masterplan.

OBJECTIVES

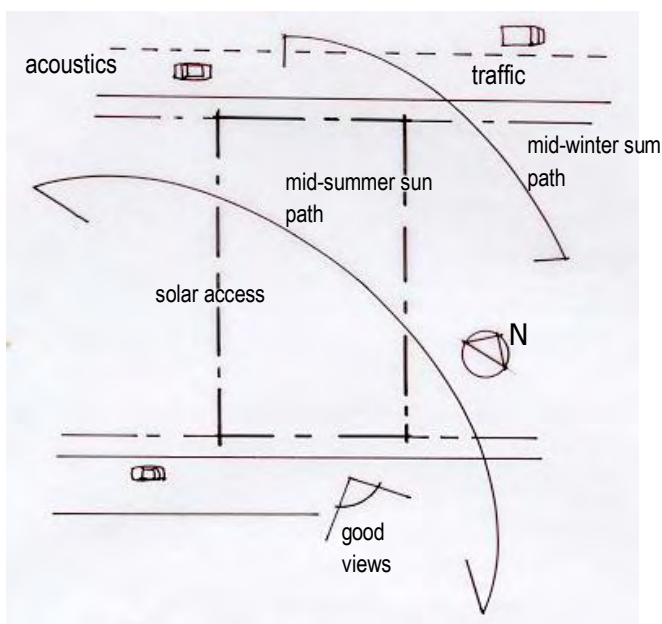
The Business Lands controls seek to:

- improve the urban form of business centres; and
- provide measures to enhance the natural and built environment.

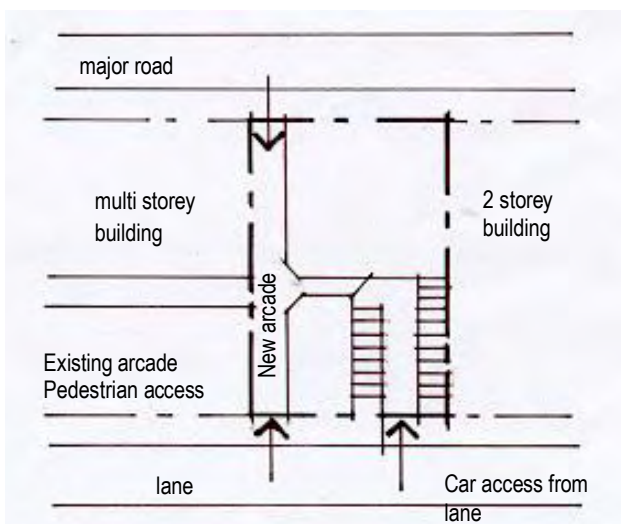
SITE ANALYSIS

Site analysis is required to identify opportunities and constraints for building design.

SITE ANALYSIS



DESIGN RESPONSE



DEVELOPMENT CONTROLS

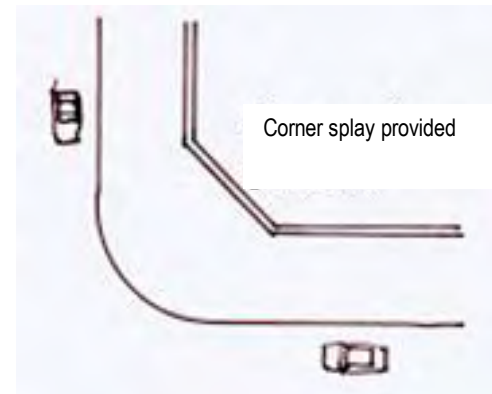
BUILDING HEIGHT

Buildings are not to exceed 11 metres in height. Underground car parking should not protrude more than 1m above natural ground level.

Note: building height (or height of building), at any point of a building, means the vertical distance between that point at ground level (existing) and the highest point of the building immediately above that point, including plant and lift overruns, but excluding communication devices, antennae, satellite dishes, masts, flagpoles, chimneys, flues and the like.

BUILDING DESIGN

- Building design should provide for a distinguishable top, middle, base and entry.



- On corner sites and sites where vistas are terminated, buildings should include special architectural emphasis acknowledging the corner or vista.
- Buildings should be designed to take advantage of views without significantly compromising the views from surrounding buildings, in particular residential buildings.
- Provide awnings or colonnades for shelter from rain and the sun; these should align with adjoining buildings.
- Make buildings energy efficient (refer to Energy Efficiency Information Sheet).

DENSITY

- The bulk, scale and intensity of development should be consistent with the aim of the zone.
- Retail development is limited to 200m² per shop with a provision for two shops to have an individual floor space of 500m².

- Development is not to result in a building that exceeds a floorspace ratio of 1:1.

Note:

Floorspace ratio means the ratio of the gross floor area of any buildings on the site to the site area.

SETBACKS

Building setbacks will be assessed on merit.

HERITAGE

- Development proposals adjacent heritage items should be accompanied by a report demonstrating the means by which the proposal will integrate with the item(s).

PARKING AND ACCESS

- Applicants are requested to submit a traffic assessment with the development application.
- Off street car parking areas are to be provided in accordance with the Off Street Car Parking section of this plan.
- Car parking areas should provide for safe and efficient ingress and egress.
- Car parking areas should be screened from public areas.
- Where rear lane access is available, this should be used for direct access to car parking areas and loading / unloading areas.
- Appropriate facilities and access should be provided for people with disabilities.

LANDSCAPING

- A landscape plan prepared by a qualified landscape architect or designer is to be submitted showing existing trees and proposed landscaping.
- Landscaping is to be provided in the front setback areas (if applicable), in parking areas and in rear setbacks visible from streets, service roads, railway lines or residential development.
- Incorporate signs into landscaping, where appropriate.
- Refer to Landscaping Information Sheet for guidelines on landscape planting and species selection.

LOADING / UNLOADING BAYS

- Bays should be located such that vehicles do not utilise any public road, footway, laneway, or service road when loading / unloading.
- Bays and turning areas should have dimensions designed in accordance with the size of vehicles that will service the site.

OFF-SITE INFRASTRUCTURE REQUIREMENTS

Site frontages are to have the following minimum standards to suit this type of development. Developers will be required to upgrade site frontages, at the developer's full cost (including design), to meet these standards upon development of the property:

- sealed road pavement;
- concrete kerb and gutter;
- concrete footpath to the nearest cross street;
- piped stormwater drainage;
- if access is to be via a lane, the lane is to be constructed full width from at least one street.

SERVICES

Waste

- Facilities are to be provided to meet the trade waste and recyclables storage needs generated on the premises.
- Where excavation is proposed, sites for disposal of excavated material are to be nominated in the proposal for Council approval.
- Trade waste facilities are to be designed to comply with Council's Trade Waste Policy.

Water and Sewerage Services

- Water and sewer connections, where not available to a lot, will require the extension of Council's main to service that lot.
- Design plans are to be prepared by a suitably qualified Engineer.

Stormwater

All stormwater is to be directed to the street drainage system, or to an inter-allotment drainage easement where available. Surface water is not to be directed to neighbouring properties. Stormwater to kerb connections are to be via kerb adaptor units.

OFF-STREET CAR PARKING

This section of the DCP applies to the entirety of the area covered by the North Coffs DCP, as shown on Map 1 Masterplan.

OBJECTIVES

The controls in this section of the DCP seek to:

- provide detailed parking requirements for individual land use categories; and
- ensure car parking areas relate to site conditions and meet user needs.

DESIGN AND SAFETY

Car parking areas shall be constructed to an all weather hard stand surface and be graded and drained to Council's stormwater system.

Motorists should easily be able to locate the car parking area. Parking spaces should be clearly marked and signposted where appropriate. Vehicles shall be able to enter and leave the site in a forward direction where a development has access along a main route, a steep site or a battle-axe site.

All car parking and manoeuvring areas are to be located on the development site and clear of the footpath and verge.

LANDSCAPING

Proposals for parking areas shall be accompanied by a landscape plan demonstrating means to provide shade and soften the visual impact of any parking structure or parking area. Setbacks shall be determined depending on the urban form; generally major car parks (typically exceeding 100 car spaces) require a continuous landscape setback averaging six metres from the property boundary.

Car parks for developments in residential areas shall not occupy the entire front of the allotment. Existing vegetation may require retention, protection and consideration in the overall car park design. Shade tree planting is required for every eight car spaces and including the boundary of the car park. The use of grass swales to capture pollutants shall be considered (see below).

Adequate area shall be provided for tree growth to accommodate the full trunk growth of the species i.e. a minimum of one metre from the mature growth diameter of the tree. Pits or diamonds are not permitted, as they are not adequate areas for tree growth. Root barrier and root deflector pits shall be included to protect the surrounding pavement. Alternative shade structures may be considered for internal areas of the car park layout where there is no adequate area for tree growth.

PARKING SPACES

The number of parking spaces required for different land uses should be provided on-site in accordance with Schedule A.

Parking spaces should be designed in accordance with Australian Standard 2890.1 and 2890.2. The attached diagram provides a guideline for designing parking areas.

Car parking for disabled persons shall be provided for business, motel and other developments where disabled access to the building is required. Parking spaces for disabled persons should have a minimum dimension of 5.4m by 3.2m and located as close as practicable to the main entrance of the building.

The minimum vertical clearance for parking areas is 2.3m.

VISITOR / OVERFLOW CAR PARKING REQUIREMENTS FOR ALL RESIDENTIAL DEVELOPMENT

Visitor / overflow car parking is to be provided at a rate of one space per every five dwellings or part thereof.

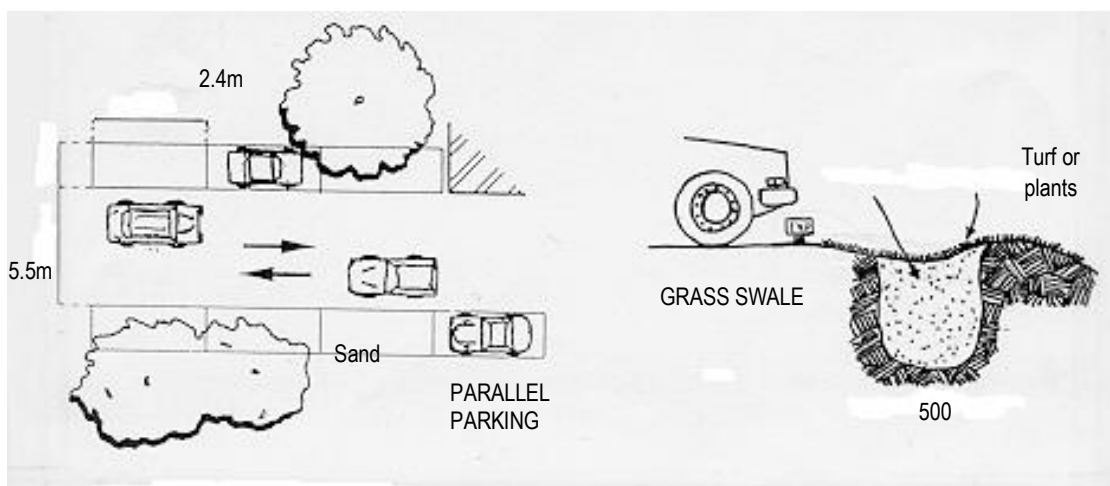
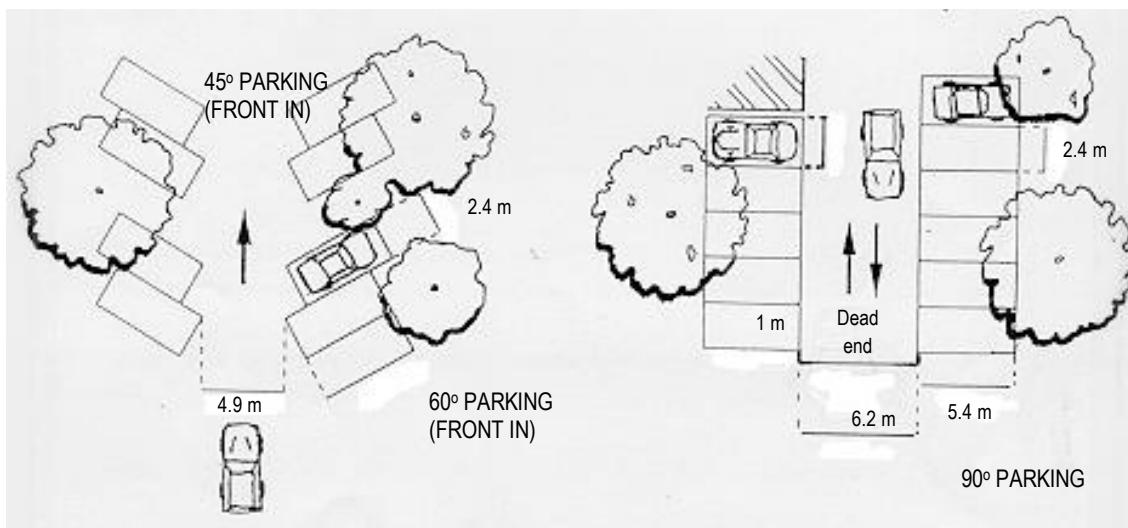
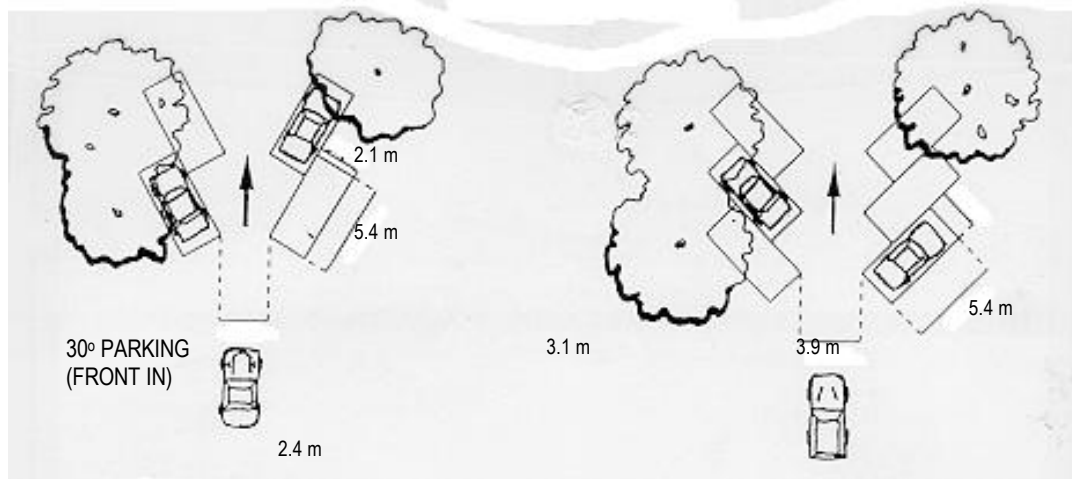
Visitor / overflow car parking is to be provided within the development site. Visitor / overflow parking is to be behind the front setback and freely accessible at all times.

Visitor / overflow car parking where proposed must be clearly detailed in the development documentation.

For additional information on car parking provisions, please refer to the Off Street Car Parking DCP.

ACCESS

Access to parking areas should be designed to minimise conflict between pedestrians, cyclists and traffic. Council may require road or traffic management works to ensure safe access to parking areas. Where developments front a busy road, access to rear lanes (if available) should be provided



SCHEDULE A - PARKING REQUIREMENTS

The following table provides a summary of the off street car parking requirements for the differing land use categories.

LAND USE	PARKING REQUIREMENTS
RESIDENTIAL	
Dwellings <ul style="list-style-type: none"> Small ($\leq 100\text{m}^2$ GFA) Large ($> 100\text{m}^2$ GFA) 	1 space (behind building line) 2 spaces (at least 1 space behind building line)
Housing for Aged and Disabled Persons <ul style="list-style-type: none"> Self Contained Units 	PUBLIC 1 space per 5 units PRIVATE 0.5 spaces per small unit ($< 55\text{m}^2$) 0.85 spaces per medium unit ($55 - < 85\text{m}^2$) 1 space per large unit ($> 85\text{m}^2$)
<ul style="list-style-type: none"> Hostels 	PUBLIC OR PRIVATE 1 space per 10 beds + 1 space per 2 employees + 1 space per ambulance
COMMERCIAL AND RETAIL	
Business and Office Premises	1 space per 40m^2 GFA
Shops	1 space per 23m^2 GLFA
Service Stations and convenience stores	Requirements are additive: 6 spaces per work bay (Note: stack parking acceptable) 1 space per 20m^2 GFA of convenience store <i>If restaurant present, then greater of:</i> 1 space per 6.6m^2 GFA, or 1 space per 3 seats)
REFRESHMENTS	
Restaurants	For new development sites: 1 space per 6.6m^2 GFA For change of use of existing premises: 1 space per 23m^2 GFA
RECREATIONAL AND TOURIST FACILITIES	
Recreational facilities <ul style="list-style-type: none"> Squash courts Tennis courts Bowling alleys Bowling greens Gymnasiums 	3 spaces per court 3 spaces per court 3 spaces per alley 30 spaces for first green + 15 spaces for each additional green Regional centres 1 space per 33m^2 GFA Sub-regional centres 1 space per 22.2m^2 GFA (minimum)

LAND USE	PARKING REQUIREMENTS
RECREATIONAL AND TOURIST FACILITIES	
Tourist facilities <ul style="list-style-type: none"> • Motels • Hotels • Caravan parks • Marinas 	<p>1 space per unit + 1 space per 2 employees <i>If restaurant included then add the greater of:</i> 1 space per 13.2m² GFA of restaurant / function room, or 1 space per 6 seats + 1 space for managers residence Subject to parking study (see Note 4)</p> <p>1 space per caravan and tent site + 1 visitor space per 10 long term sites and 1 per 20 short term sites 1 visitor space per 40 tent sites A minimum of 4 visitor spaces must be provided in any caravan park</p> <p>If a survey of a similar existing development has not been undertaken, the following figures may serve as a general guide: 0.6 spaces per wet berth 0.2 spaces per dry storage berth 0.2 spaces per swing mooring 0.5 spaces per marina employee</p>
HEALTH AND COMMUNITY SERVICES	
Child care centres	1 space per staff member + the provision of a safe set-down and pick-up area of 1 space per 10 children, with a minimum of 2 spaces
OTHER	
Other developments not specified	Subject to Parking Study (see Note 4)

Note: Depending on land use type, parking for delivery / service vehicles, courier vehicles, bicycles, buses, taxis should also be provided.

1. For mixed developments the number of car parking spaces should be calculated on the basis of each separate use e.g. shops with housing above would be calculated on the basis of the number of dwellings and gross floor area.
2. Calculations should be rounded up to the nearest whole number e.g. if the calculation determines that 2.3 spaces are required then 3 spaces would be required.
3. Where developments are subject to a parking study, the applicant will be required to undertake a parking study of a similar type of development in a similar location to determine the number of parking spaces required for the proposed development. Parking studies must be undertaken by a suitably qualified professional.
4. GFA – Gross Floor Area – see glossary for definition.

GLOSSARY

Gross floor area (GFA): the sum of the areas of each floor of a building where the area of each floor is taken to be the area within the outer face of the external enclosing walls as measured at a height of 1,400mm above each floor level excluding:

- columns, fin walls, sun control devices and any elements, projections or works outside the general line of the outer face of the external wall;
- lift towers, cooling towers, machinery and plant rooms, and ancillary storage space and vertical air-conditioning ducts;
- car parking areas which meet requirements of council and internal access thereto; and
- space for the loading and unloading of goods.

Gross leasable floor area (GLFA): the sum of the area of each floor of a building where the area of each floor is taken to be the area within the internal faces of the walls, excluding stairs, amenities, lifts, corridors and other public areas but including stock storage area.

SUBDIVISION

This section of the DCP applies to the entirety of the area covered by the North Coffs DCP, as shown on Map 1 Masterplan.

OBJECTIVES

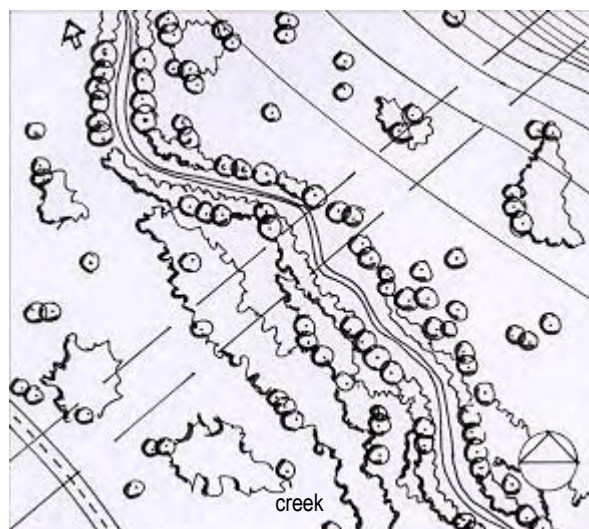
The controls in this DCP seek to:

- provide measures to protect and enhance the natural and built environment by ensuring that subdivision patterns relate to site conditions;
- ensure that subdivisions do not detract from the desired future neighbourhood character of the locality; and
- promote the orderly development of land by ensuring that the appropriate form of subdivision is used (i.e. Torrens, community, strata title) while ensuring that it is adequately serviced.

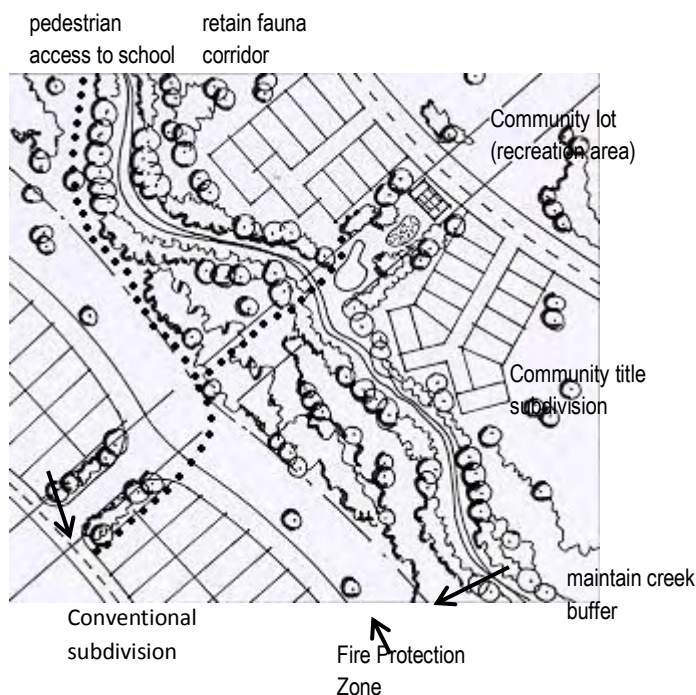
SITE ANALYSIS

- A site analysis plan is required to identify opportunities and constraints relating to the subdivision pattern and potential end use of the land.
- A site analysis plan should be prepared having regard to the following, where relevant:
 - waterways (creeks, rivers, streams);
 - significant vegetation / habitat / fauna corridors;
 - flood liable land;
 - steep land / land slip;
 - fire hazard;
 - access points (vehicles, pedestrians, cyclists);
 - soil conditions (acid sulfate, contaminated);
 - surrounding land uses;
 - service connections;
 - easements;
 - archaeological sites;
 - topography (contours to Australian Height Datum at 1m intervals);
 - aspect;
 - drainage systems;
 - existing buildings, driveways, septic tanks and disposal areas; and
 - street and lot layout of locality.

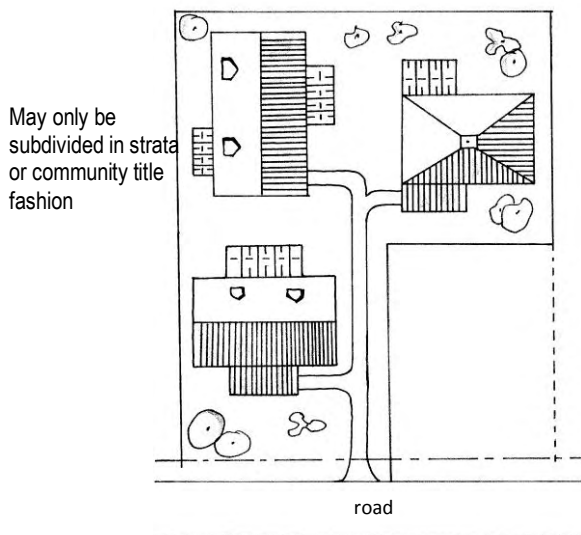
SITE ANALYSIS



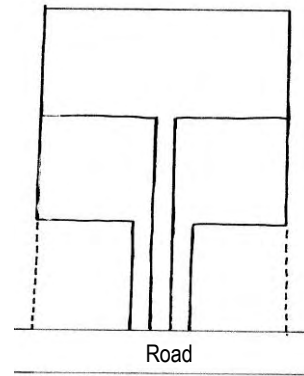
DESIGN RESPONSE



- A subdivision which will involve a lot having vehicular access to a lane will only be permitted after the lot has been substantially developed (i.e. vacant lots off laneways are not to be created), and the lot adjoining the lane is to have 2 m wide frontage fenced and paved to the primary road, to provide for pedestrian access, mailbox, services (water, sewer, electricity, communication).
- Where a subdivision will create more than two lots or two dwellings using a common driveway, then the form of subdivision is to be either strata or community title. In this situation, the common driveway is to be constructed in concrete a minimum of 4.5 m wide at the street, continuing at this width to a depth of 6 m, and thereafter of minimum width 2 m, prior to the issue of the Subdivision Certificate. Adequate room at the frontage to accommodate water meters and letterboxes is also required.



- Subdivisions are not permitted where three or more 'battle axe handles' will directly adjoin.



SERVICES

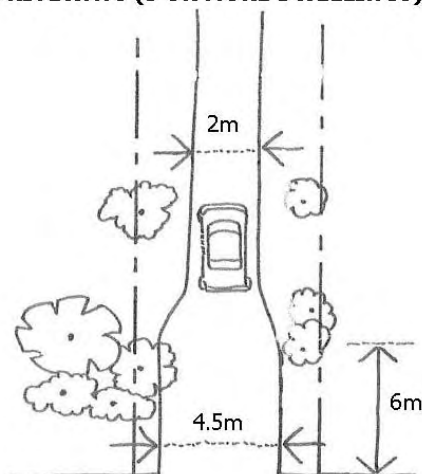
General

▪ Urban Areas

Subdivisions in urban areas are generally required to provide infrastructure to all lots including:

- road;
- footpath;
- kerb and gutter;
- drainage;
- reticulated sewer and water;
- telecommunications;
- street lighting; and
- electricity.

DRIVEWAYS (3 OR MORE DWELLINGS)



Stormwater Drainage

Stormwater drainage shall be designed and provided in accordance with the Stormwater Strategy identified in Part 2.

The design details will need to be approved by Council before the drainage is provided, and will need to be completed to Council's satisfaction prior to the issue of the Subdivision Certificate.

Stormwater is to be gravity drained to Council's drainage system. In some circumstances inter-allotment drainage easements over downstream properties may be required. This will necessitate a letter of consent from the owner(s) of the downstream properties to be submitted with the development application.

Drainage from sites should reflect the pre-existing or natural situation in terms of location, quantity, quality and velocity.

Utility Services

Utility services must be extended to all lots within a subdivision in accordance with **Table 4.5** below (except for common property in community title and strata subdivisions).

Table 4.5 Service Provision

Utility Service	Urban Area	Rural Residential Area	Rural area
Council's water main	Yes	No	No
Council's sewer main	Yes	No	No
Telephone	Yes*	Yes*	No
Electricity	Yes*	Yes*	Yes**

*** = In greenfield subdivisions these services must be underground.**

**** = Unless the applicant can demonstrate that alternative methods of providing electricity exists or that the provision of this service is cost prohibitive.**

Conditions on the development consent will outline how, when and to what standard, these services are to be provided.

STREET TREE MASTERPLAN

A Street Tree Masterplan will be required for subdivisions on greenfield sites*. The Masterplan aims to guide street tree planting, providing for a more colourful City which complements its natural setting.

*** = Where public road is proposed, and may be required for community title subdivisions.**

Planting proposed by the Masterplan is to be determined having regard to:

- site and dwelling boundaries;
- location and canopy of existing trees, noting any trees that overhang the site;
- adjacent streets and trees;
- any connection to open space networks or proposed public reserves;
- paving materials and drainage treatment;
- details of any existing fencing and walls; and
- location of underground services.

DEVELOPER CONTRIBUTIONS

In many cases the payment of contributions are required to cover the cost of services and facilities which are provided by Council. These contributions are often levied with subdivision, prior to the issue of the Subdivision Certificate.

Contributions on the development consent will indicate whether these contributions are required.

Council's authority to impose conditions for these payments is derived from the *Environmental Planning and Assessment Act 1979* and the *Water Management Act 2000*.

Ordinarily, subdivisions of residential and rural residential land will be required to pay contributions and are outlined in Council's developer contribution plans and Development Servicing Plan 2003.

SIGNS

OBJECTIVES

The controls in this DCP seek to:

- enhance visual amenity;
- provide equity for outdoor advertisers; and
- provide safety guidelines for outdoor advertisers.

PROCEDURES

This part describes the conditions under which a sign may be erected in Coffs Harbour (refer to flow chart). A proposed sign will fall into one of the following three (3) categories:

1. Signs which are Prohibited

Signs, except real estate and temporary signs, are specifically prohibited in Environmental Protection zones 7A, 7B and 7C.

2. Signs requiring Consent

All signs other than those listed in point 3 below require consent. Consent is sought through the lodgement of a development application (refer to procedures flow chart).

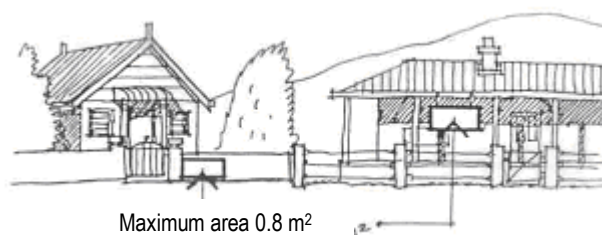
3. Signs not requiring Consent

There is no need to apply for consent if the sign falls into the categories in **Table 4.6**. This means you may erect the sign without Council approval.

Table 4.6 Signs Allowed Without Consent

Zone	Type of Sign
Residential	One residential sign per allotment Discretionary Signs
Open Space	Discretionary signs
Environmental Protection	Real Estate signs Temporary Signs

SIGNS NOT REQUIRING CONSENT - RESIDENTIAL



SIGN DICTIONARY

“discretionary advertisement” means:

- an advertisement that is not visible from outside the site where it is located;
- a **real estate sign**;
- a **temporary sign**;
- a public notice displayed by a public authority;
- advertisements behind the glass line of a shop window;
- a single **pylon sign** erected on a site lawfully used for the purpose of a tourist facility; or
- a **painted wall sign**.

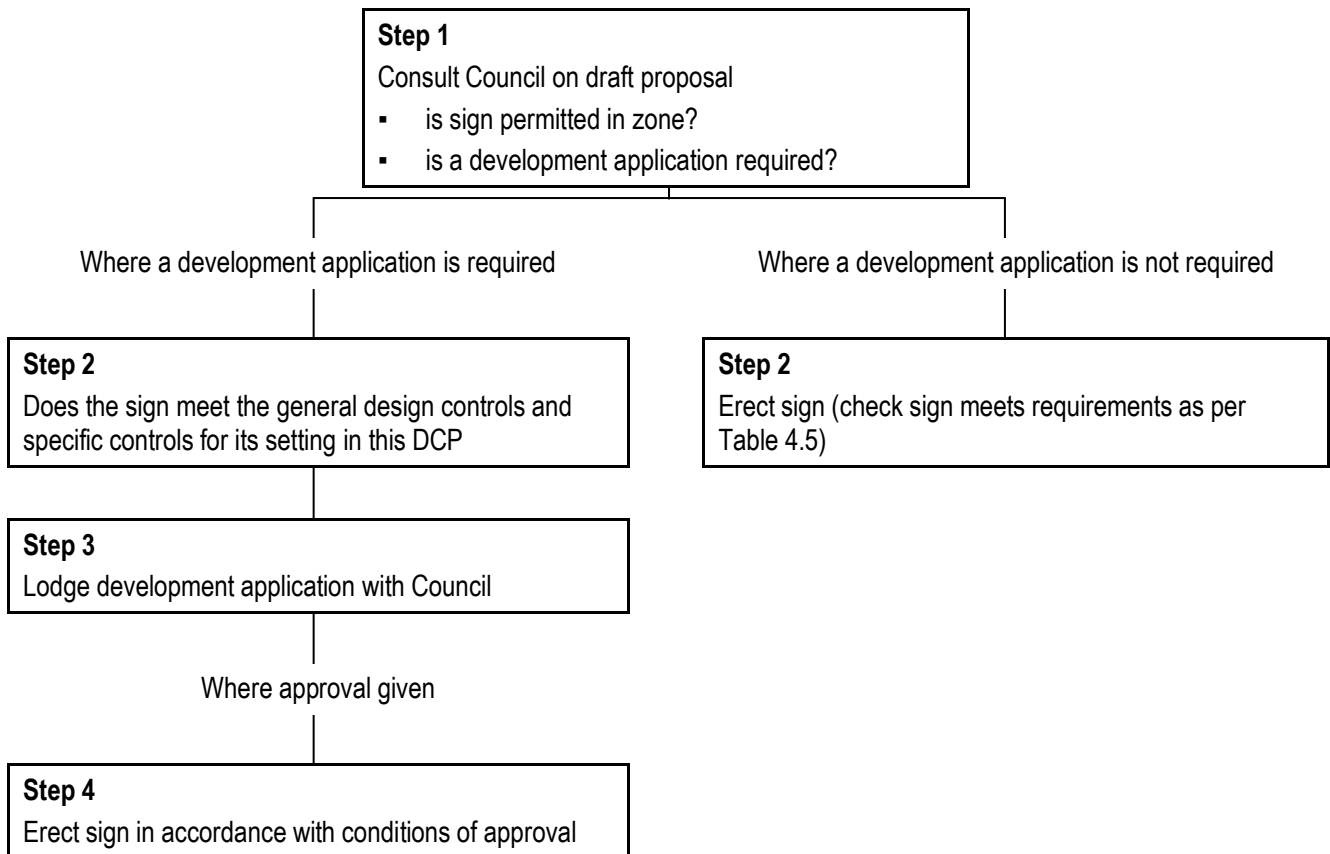
“pylon sign” means an advertisement erected on a pole (or pylon) that is independent of a building and has a height of not more than 6 m but not less than 2.7 m above ground level.

“real estate sign” means a ‘For Sale’ or ‘To Let’ sign erected at or affixed to the property the subject of the sign and:

- in the case of an advertisement in respect of residential or rural premises does not exceed 2.5 m² in area;
- in all other cases does not exceed 4.5 m² in area; and
- is not displayed for more than 14 days after sale or letting.

“residential sign” means a non-illuminated advertisement not exceeding 0.8 m² in area that indicates the name, occupation and other particular matters of the resident(s) of the premises upon which the advertisement is displayed.

PROCEDURES FLOW CHART



“temporary sign” means an advertisement of a temporary nature which:

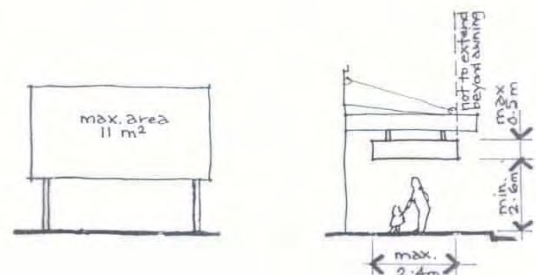
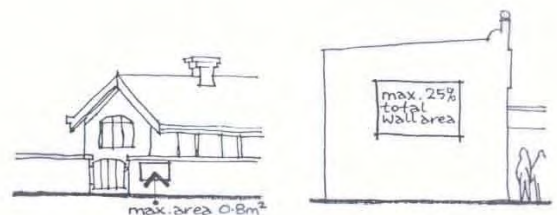
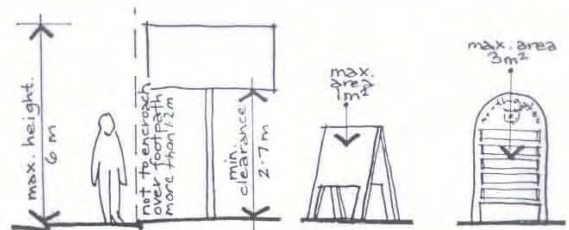
- concerns any local event of a religious, educational, cultural, political, social, or recreational nature;
- does not include advertising of a commercial nature, except for the name of any sponsor; and
- is not displayed earlier than 28 days before the commencement date of the event and is removed within 14 days after the end of the event.

SIZE OF SIGNS

The size of signs is to be in accordance with the diagram, where specified.

EXISTING SIGNS

Where a number of approved signs that would not comply with this DCP exist on a property, an endeavour is to be made to consolidate the number of signs or simplify the sign messages.



RESIDENTIAL SETTINGS

- These controls apply to residential settings:
 - signs for commercial, industrial, retail or tourist / recreation activities area:
 - to be located on the building(s) or fence associated with the use;
 - not to exceed dimension standards and design guidelines described in the general design controls;
 - not to be erected on walls facing adjoining residences; and
 - to be below the roof eaves line or parapet line and relate to the architectural appearance of the building and its environs.
 - signs for home occupations and home industries are limited to an area of 0.8 m² and are to indicate the name and occupation and other related matters of the resident only.
 - No more than one permanent sign per site should be erected.

RESIDENTIAL



NOTIFICATION

OBJECTIVES

- To encourage public participation in the development control process; and
- To inform the community about potentially sensitive developments.

WHEN WILL DEVELOPMENT BE NOTIFIED / ADVERTISED?

General

- Adjoining landowners will be notified if in the opinion of Council the enjoyment of land (see note) adjoining the development may be detrimentally affected by the proposed development.
- Owners of land other than adjoining land will be notified by advertisement in a local newspaper circulating in the Council area where in the opinion of Council (see note), the enjoyment of that land may be detrimentally affected by the proposed development.

Note:

Council will consider the following in forming its opinion as to whether or not the enjoyment of land may be detrimentally affected by a proposed development:

- ***the views to and the views from the land***
- ***the overshadowing of the land***
- ***the privacy of the land***
- ***the likelihood of the land being detrimentally affected by the proposed use***
- ***the streetscape.***

The following development is not usually notified:

- ***dwellings of single storey construction***
- ***minor alterations / additions including internal work***
- ***development in rural areas***
- ***private swimming pools or outbuildings.***

- Public authorities that Council considers may have an interest in the determination of the application will be notified.

Designated Development

Designated development is advertised in accordance with the Environmental Planning and Assessment Act Regulations.

Integrated Development

Integrated Development involving an approval:

- under Section 58 of the Heritage Act 1977;
- under the Water Act 1912; and
- under the Pollution Control Act 1970,

is advertised in accordance with the Environmental Planning and Assessment Act Regulations.

State Development

State Development is advertised in accordance with the Environmental Planning and Assessment Act Regulations.

Notes:

- ***A notice to an association for a community, precinct or neighbourhood parcel within the meaning of the Community Land Development Act 1989 or to a body corporate for a parcel within the meaning of the Strata Titles Act 1973 or the Strata Titles (Leasehold) Act 1986 is taken to be a notice under this section to the owner of each lot within the parcel concerned.***
- ***If a parcel of adjoining land is owned by more than one person, a notice to one owner is taken to satisfy the requirements of this section.***
- ***Notice is not required to be given to an owner of adjoining land if that owner is the person, or one of the persons, who made the application for approval to erect the building.***

CONTAMINATED LAND

Category 1 remediation work, as defined in State Environmental Planning Policy No. 55 - Remediation of Land, is advertised in accordance with the Environmental Planning and Assessment Act Regulations for a period of 30 days.

A notice of completion of a category 1 remediation work must be provided to Council (with a copy to the consent authority if Council was not the consent authority) within 30 days after the completion of the work.

Notice must be given to the Council, at least 30 days prior to the commencement of category 2 remediation work.

FORM OF NOTICE / ADVERTISEMENT

The following is to be included in a written notice or published advertisement:

- description of the land (including address);
- name of applicant;
- name of consent authority;
- description of development;
- a statement, including the dates, that the application and any accompanying documents may be inspected at Council's office during working hours; and
- a statement that any person during the exhibition period may make a written submission.

In respect of category 2 remediation work of contaminated land, the notice must be in writing and:

- provide the name, address and telephone number of the person who has the duty of ensuring that the notice is given;
- briefly describe the remediation work;
- show why the work is category 2 remediation work;
- provide a property description, street address and map of the location of the land; and
- provide estimates of the dates for the commencement and completion of the work.

Note:

Minimum exhibition period is 14 days (30 days for integrated development).

Where proposal is for integrated development, the notice must contain a statement that it is integrated development and must state the approvals required and relevant approval bodies.

Council may decide to dispense with further notification / advertising in relation to the amended, substituted or later application.

NOTICE OF DETERMINATION

All persons making written submissions will be given written notice of the determination of the application.

NOTIFICATION / ADVERTISING FEE

A fee to cover Council's costs for notifying or advertising in relation to this DCP may be charged on each application.

CIRCUMSTANCES IN WHICH NOTICE REQUIREMENTS MAY BE DISPENSED WITH

If:

- a development application is amended, or substituted, or withdrawn and later replaced before it has been determined by Council; and
- Council has notified / advertised the original application, in accordance with the format in this DCP; and
- Council is of the opinion that the amended, substituted or later application differs only in minor respects from the original application,

Coffs Harbour City Council
Cnr Coffs and Castle Streets
(Locked Bag 155)
COFFS HARBOUR NSW 2450

Telephone: (02) 6648 4000
Website: www.coffsharbour.nsw.gov.au